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June 16, 2016

236193

Cynthia T. Brown
Chief of the Section of Administration, Office of Proceedings
Surface Transportation Board
395 E Street, S.W.
Washington, D.C. 20423

ENTERED
Office of Proceedings
June 16, 2014
Part of
Public Record

Re: **Docket No. AB-55 (Sub-No. 709X), CSX Transportation, Inc.—Abandonment
Exemption—in Marion County, IN**

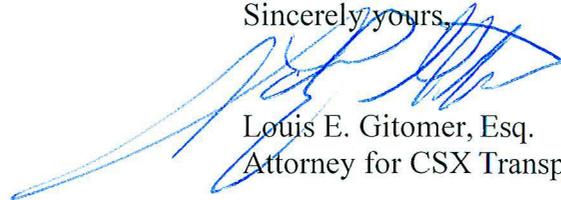
Dear Ms. Brown,

Enclosed are the original and 10 copies of a Verified Notice of Exemption for CSX Transportation, Inc. (“CSXT”) to abandon an approximately 1.49-mile rail line on the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, between north of West 29th Street, milepost QIY 3.69, and the end of track south of Langsdale Avenue, milepost QIY 2.20, in Indianapolis, Marion County, IN. Color Exhibits are at the end of the Notice. Also enclosed are a check for the filing fee of \$3,900, and a computer diskette containing the Notice in WORD and pdf format.

Please time and date stamp the additional copy of this letter and Notice and return it in the enclosed prepaid envelope.

Thank you for your assistance. If you have any questions, please contact me.

Sincerely yours,



Louis E. Gitomer, Esq.
Attorney for CSX Transportation, Inc.

Enclosure

FILED
June 16, 2014
SURFACE
TRANSPORTATION BOARD

FEE RECEIVED
June 16, 2014
SURFACE
TRANSPORTATION BOARD

BEFORE THE
SURFACE TRANSPORTATION BOARD

Docket No. AB-55 (Sub-No. 709X)

CSX TRANSPORTATION, INC.—ABANDONMENT EXEMPTION—
IN MARION COUNTY, IN

VERIFIED NOTICE OF EXEMPTION

Steven C. Armbrust, Esq.
CSX Transportation, Inc.
500 Water Street
Jacksonville, FL 32202
(904) 359-1229

Louis E. Gitomer, Esq.
Melanie B. Yasbin, Esq.
Law Offices of Louis E. Gitomer, LLC
600 Baltimore Avenue, Suite 301
Towson, MD 21204
(410) 296-2250
Lou@lgrailaw.com

Attorneys for: CSX TRANSPORTATION,
INC.

Dated: June 16, 2014

BEFORE THE
SURFACE TRANSPORTATION BOARD

Docket No. AB-55 (Sub-No. 709X)

CSX TRANSPORTATION, INC.—ABANDONMENT EXEMPTION—
IN MARION COUNTY, IN

VERIFIED NOTICE OF EXEMPTION

CSX Transportation, Inc. (“CSXT”) files this Verified Notice of Exemption pursuant to the class exemption at 49 C.F.R. §1152.50 for CSXT to abandon an approximately 1.49-mile rail line on the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, between north of West 29th Street, milepost QIY 3.69, and the end of track south of Langsdale Avenue, milepost QIY 2.20, in Indianapolis, Marion County, IN (the “Line”). After receiving abandonment authority, CSXT plans to abandon the Line, salvage the track and materials and sell or lease the real estate. No local rail traffic has moved over the Line during the past two years. Any overhead traffic on the Line can be and has been rerouted.

Based on information in the possession of CSXT, the Line does not contain federally granted rights-of-way. Any documentation in the railroad’s possession will be made available promptly to those requesting it.

1. Proposed consummation date. The proposed consummation date is August 5, 2014.

2. Certification required by 49 C.F.R. §1152.50(b). *See* Exhibit A.
3. Information required by 49 C.F.R. §1152.22(a)(1) through (4), (7), and (8), and (e)(4).

(a) General.

(1) Exact name of applicant. CSX Transportation, Inc.

(2) Whether applicant is a common carrier by railroad subject to 49 U.S.C.

Subtitle IV, Chapter 105. CSXT is a common carrier by railroad subject to 49 U.S.C. Subtitle IV, Chapter 105.

(3) Relief sought. CSXT seeks to use the class exemption at 49 C.F.R. §1152.50 to abandon an approximately 1.49-mile rail line on the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, between north of West 29th Street, milepost QIY 3.69, and the end of track south of Langsdale Avenue, milepost QIY 2.20, in Indianapolis, Marion County, IN. The Indianapolis Station is at FSAC 75128 and OPSL 45400, but will not be closed as a result of the abandonment.

(4) Map. *See* Exhibit B.

(7) Name, title, and address of representative of applicant to whom correspondence should be sent. Louis E. Gitomer, Law Offices of Louis E. Gitomer, LLC, 600 Baltimore Avenue, Suite 301, Towson, MD 21204, (410) 296-2250, Lou@lgraillaw.com.

(8) List of all United States Postal Service ZIP Codes that the line proposed for abandonment traverses. The Line traverses U.S. Postal ZIP Code 46208.

(e) Rural and community impact.

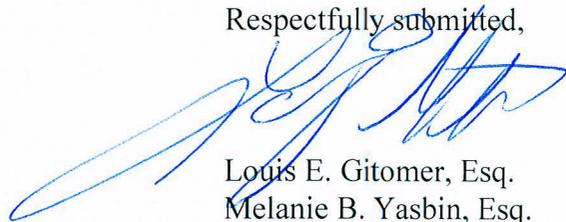
(4) Statement of whether the properties proposed to be abandoned are appropriate for use for other public purposes, including roads and highways, other forms of mass

transportation, conservation, energy production or transmission, or recreation. If the applicant is aware of any restriction on the title to the property, including any reversionary interest, which would affect the transfer of title or the use of property for other than rail purposes, this shall be disclosed.

The Line may be suitable for other public purposes or trail use, but may be subject to reversionary interests.

4. The level of labor protection. The interests of any railroad employees who may be adversely affected by the proposed abandonment will be adequately protected by the labor protective conditions in *Oregon Short Line R. Co. –Abandonment–Goshen*, 360 I.C.C. 91 (1979).
5. Certification. Certificates of compliance with the notice requirements of 49 C.F.R. §§ 1152.50(d)(1) and 1105.11 are attached as Exhibit C.
6. Environmental Report. See Exhibit D.
7. Historic Report. See Exhibit E.
8. Color Exhibits. See Exhibit F.

Respectfully submitted,



Steven C. Armbrust, Esq.
CSX Transportation, Inc.
500 Water Street
Jacksonville, FL 32202
(904) 359-1229

Louis E. Gitomer, Esq.
Melanie B. Yasbin, Esq.
Law Offices of Louis E. Gitomer
600 Baltimore Avenue, Suite 301
Towson, MD 21204
(410) 296-2250
Lou@lgraillaw.com

Attorneys for: CSX TRANSPORTATION,
INC.

Dated: June 16, 2014

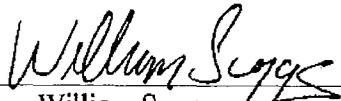
EXHIBIT A-VERIFICATION AND CERTIFICATION

**VERIFICATION AND CERTIFICATION THAT RAIL LINE MEETS
CRITERIA OF 49 C.F.R. SECTION 1152.50(b)**

STATE OF FLORIDA)
)
COUNTY OF DUVAL) ss.

I, William Scaggs state that I am Manager of Cost and Economic Analysis for CSX Transportation, Inc. ("CSXT"); that I am authorized to make this verification; and that I have read the foregoing Notice of Exemption and know the facts asserted therein are true and accurate as stated, to the best of my knowledge, information, and belief.

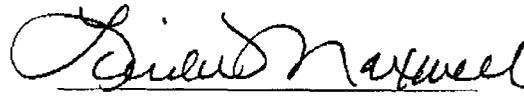
I hereby certify that no freight traffic has moved over CSXT's 1.49 mile rail line on its Northern Region, Great Lakes Division, Indianapolis Subdivision, between north of West 29th Street, milepost QIY 3.69, and the end of track south of Langsdale Avenue, milepost QIY 2.20, in Indianapolis, Marion County, IN, for two years prior to the date hereof, and that any overhead freight traffic on the line can be rerouted over other lines. Further, no formal complaint filed by a user of rail service on the line (or a State or local government entity acting on behalf of such user) regarding cessation of service over the line either is pending with the Surface Transportation Board or any U.S. District Court or has been decided in favor of a complainant within the two-year period. The foregoing certification is made on behalf of CSXT by the undersigned after due and careful investigation of the matters herein certified and based on the best of the knowledge, information, and belief of the undersigned.



William Scaggs

SUBSCRIBED AND SWORN TO before me this 13 day of June 2014.

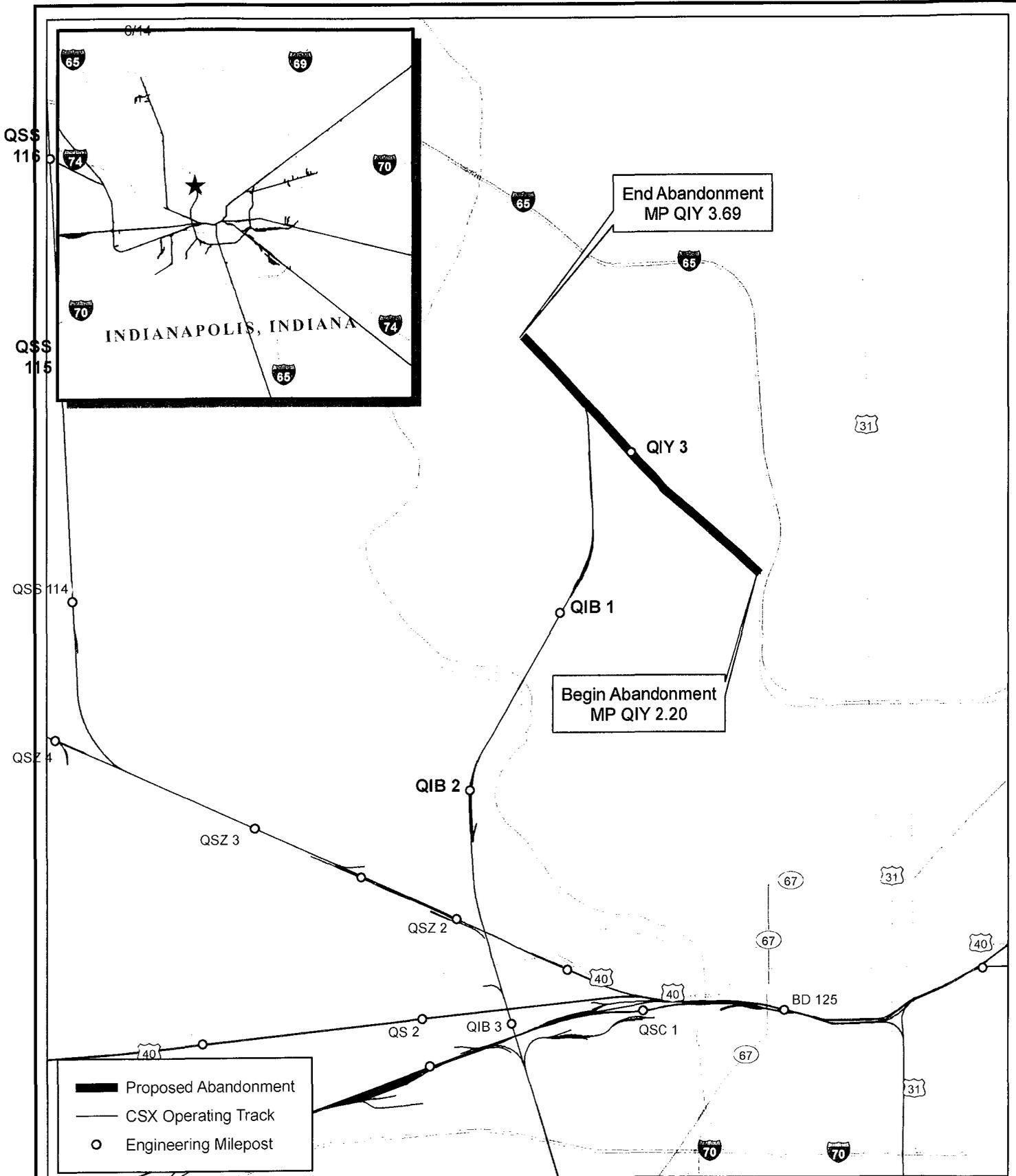
My Commission Expires: _____



Notary Public



EXHIBIT B-MAP



CSX Real Property, Inc.

CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB 55 (Sub-No. 709X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana



Feet



EXHIBIT C-CERTIFICATES OF SERVICE AND PUBLICATION

Pursuant to 49 C.F.R. § 1152.50 (d) (1), the undersigned hereby certifies that notice of the proposed abandonment in Docket No. AB-55 (Sub-No. 709X) was mailed via first class mail on June 6, 2014, to the following parties:

State Public Service Commission

Mr. Mike Riley
Manager-Rail Office
Indiana Department of Transportation
100 N. Senate Ave., ICGN 955
Indianapolis, IN 46204

Military Traffic Management Command

Headquarters
Military Surface Deployment and Distribution Command
Transportation Engineering Agency
ATTN: SDTE-SA (Railroads for National Defense)
1 Soldier Way, Building 1900W
Scott AFB, IL 62225

National Park Service

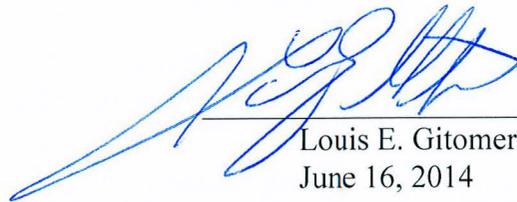
Mr. Rick Potts
Chief, Conservation and Outdoor Recreation Division
Rivers, Trails and Conservation Assistance Program National Park Service
1849 C Street, N.W. (Org Code 2220)
Washington, DC 20240

National Park Service

U.S. Department of Interior
National Park Service
Land Resources Division
1849 C Street, N.W.
Washington, DC 20240

U.S. Department of Agriculture

U.S. Department of Agriculture
Chief of the Forest Service
1400 Independence Avenue, S.W.
Washington, DC 20250-0003



Louis E. Gitomer
June 16, 2014

CERTIFICATE OF PUBLICATION

The undersigned hereby certifies that notice of the proposed abandonment in Docket No. AB-55 (Sub-No. 709X) in the following form was advertised on June 12, 2014 in The Indianapolis Star, a newspaper of general circulation in Marion County, IN as required by 49 C.F.R. §1105.7(c) and .12.



Louis E. Gitomer
June 16, 2014

Notice of Intent To Abandon

CSX Transportation, Inc. (“CSXT”) gives notice that on or about June 16, 2014, it intends to file with the Surface Transportation Board, Washington, DC 20423, a notice of exemption under 49 CFR 1152 Subpart F—Exempt Abandonments permitting the abandonment of an approximately 1.49-mile rail line on the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, between north of West 29th Street, milepost QIY 3.69, and the end of track south of Langsdale Avenue, milepost QIY 2.20, in, which traverses United States Postal Service ZIP Code 42608 in Indianapolis, Marion County, IN (the “Line”). The proceeding has been docketed as No. AB-55 (Sub-No. 709X).

The Board’s Office of Environmental Analysis (OEA) will generally prepare an Environmental Assessment (EA), which will normally be available 25 days after the filing of the notice of exemption. Comments on environmental and energy matters should be filed no later than 15 days after the EA becomes available to the public and will be addressed in a Board decision. Interested persons may obtain a copy of the EA or make inquiries regarding environmental matters by writing to the OEA, Surface Transportation Board, Washington, DC 20423 or by calling that office at (202) 245-0295.

Appropriate offers of financial assistance to continue rail service can be filed with the Board. Requests for environmental conditions, public use conditions, or rail banking/trails use also can be filed with the Board. An original and 10 copies of any pleading that raises matters other than environmental issues (such as trails use, public use, and offers of financial assistance) must be filed directly with the Board’s Section of Administration, Office of Proceedings, 395 E Street, SW, Washington, DC 20423–0001 [See 49 CFR 1104.1(a) and 1104.3(a)], and one copy must be served on applicant’s representative [See 49 CFR 1104.12(a)]. Questions regarding offers of financial assistance, public use or trails use may be directed to the Board’s Office of Public Assistance, Governmental Affairs, and Compliance at (202) 245-0238. Copies of any comments or requests for conditions should be served on the applicant’s representative: Louis E. Gitomer, 600 Baltimore Avenue, Suite 301, Towson, MD 21204, (410)296-2250, Lou@lgrailaw.com.

EXHIBIT D- ENVIRONMENTAL REPORT

ENVIRONMENTAL REPORT CERTIFICATE OF SERVICE

Pursuant to the requirements of 49 C.F.R. §1105.7, the undersigned hereby certifies that a copy of the Environmental Report in Docket No. AB-55 (Sub-No. 709X) was mailed via first class mail on April 25, 2014 to the following parties:

Indiana Department of Environmental Management
Indiana Government Center
Room 1207
100 N. Senate Avenue, MC 50-07
Indianapolis, In 46204

U.S. Fish & Wildlife Service
Great Lakes-Big Rivers Region
620 South Walker Street
Bloomington, IN 47403-2121

Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room 274
Indianapolis, IN 46204

Environmental Protection Agency
Region 5
Ralph Metcalfe Federal Building
77 West Jackson Boulevard
Chicago, IL 60604-3590

Indianapolis and Marion County
Department of Metropolitan Development
City-County Building
200 East Washington Street, Suite 2042
Indianapolis, IN 46204

United States Department of Agriculture
Natural Resources Conservation Service
Indiana State Office
6013 Lakeside Boulevard
Indianapolis, IN 46278-2933

Mayor Gregory A. Ballard
2501 City-County Building
200 East Washington Street
Indianapolis, IN 46204

U.S. Army Corps of Engineers
Louisville District-Mazzoli Federal Building
600 Martin Luther King, Jr. Place
Louisville, KY 40202

Division of Water
Indiana Department of Natural Resources
402 West Washington Street, Room 264
Indianapolis, IN 46204

National Park Service
1849 C Street NW
Washington, DC 20240

Indiana Department of Transportation
Greenfield District
32 South Broadway
Greenfield, IN 46140

National Geodetic Survey
NGS.InfoCenter@noaa.gov



Louis E. Gitomer
June 16, 2014

ENVIRONMENTAL REPORT

CSX TRANSPORTATION, INC. Indianapolis, Marion County, Indiana Docket AB-55 (Sub-No. 709X)

The following information is provided in accordance with 49 C.F.R. Section 1105.7:

(1) PROPOSED ACTION AND ALTERNATIVES

Describe the proposed action, including commodities transported, the planned disposition (if any) of any rail line and other structures that may be involved, and any possible changes in current operations or maintenance practices. Also describe any reasonable alternatives to the proposed action. Include a readable, detailed map and drawings clearly delineating the project.

CSXT proposes to abandon approximately 1.49 miles of its rail line from Railroad Milepost QIY 2.20 to the end of track located at Railroad Milepost QIY 3.69 also known as the North Street Industrial Track in Indianapolis, Marion County, Indiana.

The above referenced 1.49-mile line has not generated any originating or terminating traffic in over two (2) years.

Abandonment of this line will result in the removal of the rail, crossties, and possibly the upper layer of ballast. CSXT does not intend to disturb any sub grade or sub grade structures. Finally, the operations and maintenance of this line will cease.

The only alternative would be not to abandon and to pass the opportunity costs of retaining the line to all other CSXT customers. This would not be a prudent utilization of carrier resources.

A map which delineates the proposed project is attached. (See Attachment 1)

Further attached is a distribution list of all parties who have received a copy of this report. (See Attachment 2)

(2) TRANSPORTATION SYSTEM

Describe the effects of the proposed action on regional or local transportation systems and patterns. Estimate the amount of traffic (passenger or freight) that will be diverted to other transportation systems or modes as a result of the proposed action.

There is no passenger or freight traffic on this line. Since there has not been any traffic on this line in over two (2) years, there will be no effect on existing regional or local transportation systems or patterns.

(3) LAND USE

- (i) **Based on consultation with local and/or regional planning agencies and/or a review of the official planning documents prepared by such agencies, state whether the proposed action is consistent with existing land use plans. Describe any inconsistencies.**

Applicant has not received a response to its letters dated March 27, 2014 to the Indiana Department of Transportation, Indianapolis and Marion County Department of Metropolitan Development or from the Office of the Mayor Gregory A. Ballard requesting information regarding this statement. (See Attachments 3, 4 and 5)

Based on the fact that the line has not generated any traffic in over two (2) years, Applicant believes the proposed action is not inconsistent with local land use plans.

- (ii) **Based on consultation with the U. S. Soil Conservation Service, state the effect of the proposed action on any prime agricultural land.**

Applicant received a response dated April 1, 2014 from The United States Department of Agriculture, Natural Resources Conservation Service stating *"the proposed project will not cause a conversion of prime farmland.* (See Attachment 6)

- (iii) **If the action affects land or water uses within a designated coastal zone, include the coastal zone information required by 1105.9.**

Applicant has not received a response to its March 27, 2014 inquiry to the Department of Water, Indiana Department of Natural Resources requesting information regarding this statement. (See Attachment 7)

Applicant feels that the simple removal of track material should not

have an adverse impact on any coastal zone, land or water uses.

- (iv) **If the proposed action is an abandonment, state whether or not the right of way is suitable for alternative public use under 49 U.S.C. 10906 and explain why.**

The properties proposed to be abandoned may be suitable for other public purposes, but may be subject to reversionary interests that may affect transfer of title for other than rail purposes.

(4) ENERGY

- (i) **Describe the effect of the proposed action on transportation of energy resources.**

The proposed action will have no effect on the movement and/or recovery of energy resources.

- (ii) **Describe the effect of the proposed action on recyclable commodities.**

The proposed action will have no effect on the movement and/or recovery of recyclable commodities.

- (iii) **State whether the proposed action will result in an increase or decrease in overall energy efficiency and explain why.**

The proposed action will not result in an increase or decrease in overall energy efficiency.

- (iv) **If the proposed action will cause diversions from rail to motor carriage of more than: (A) 1,000 rail carloads a year; or (B) an average of 50 rail carloads per mile per year for any part of the affected line, quantify the resulting net change in energy consumption and show the data and methodology used to arrive at the figure given.**

There will be no diversion of rail traffic to motor carriage.

(5) AIR

- (i) **If the proposed action will result in either: (A) an increase in rail traffic of at least 100% (measured in gross ton miles annually) or an increase of at least eight trains a day on any segment of rail line affected by the proposal, or (B) an increase in rail yard activity of at least 100% (measured by carload activity), or (C) an average increase in truck traffic of more than 10% of the average daily traffic or 50 vehicles a day on any affected road segment, quantify the anticipated effect on air emissions.**

The above thresholds will not be exceeded.

- (ii) If the proposed action affects a class I or non-attainment area under the Clean Air Act; and will it result in either: (A) an increase in rail traffic of a least 50% (measured in gross ton miles annually) or an increase of at least three trains a day on any segment of rail line, or (B) an increase in rail yard activity of a least 20% (measured by carload activity), or (C) an average increase in truck traffic of more than 10% of the average daily traffic or 50 vehicles a day on a given road segment, then state whether any expected increased emissions are within the parameters established by the State Implementation Plan.

The above thresholds will not be exceeded.

- (iii) If transportation of ozone depleting materials (such as nitrogen oxide and freon) is contemplated, identify: the materials and quantity, the frequency of service; safety practices (including any speed restriction); the applicant's safety record (to the extent available) on derailments, accidents and spills; contingency plans to deal with accidental spills; and the likelihood of an accidental release of ozone depleting materials in the event of a collision or derailment.

Not applicable.

(6) NOISE

If any of the thresholds identified in item (5)(i) of this section are surpassed, state whether the proposed action will cause: (i) an incremental increase in noise levels of three decibels Ldn or more, or (ii) an increase to a noise level of 65 decibels Ldn or greater. If so, identify sensitive receptors (e.g., schools, libraries, hospitals, residences, retirement communities, and nursing homes) in the project area, and quantify the noise increase for these receptors if the thresholds are surpassed.

The above thresholds will not be exceeded.

(7) SAFETY

- (i) Describe any effects of the proposed action on public health and safety (including vehicle delay time at railroad grade crossings).

Applicant believes that the abandonment will result in improvements to public safety by the elimination of seven (7) public, at-grade road crossings and one (1) private, at-grade road crossings.

- (ii) If hazardous materials are expected to be transported, identify: the materials and quantity; the frequency of service; whether chemicals are

being transported that, if mixed, could react to form more hazardous compounds; safety practices (including any speed restrictions); the applicant's safety record (to the extent available) on derailments, accidents and hazardous spills; the contingency plans to deal with accidental spills; and the likelihood of an accidental release of hazardous materials.

Not applicable.

- (iii) **If there are any known hazardous waste sites or sites where there have been known hazardous material spills on the right of way, identify the location of those sites and the types of hazardous materials involved.**

Applicant's records do not indicate any known hazardous material spills or hazardous waste sites.

(8) BIOLOGICAL RESOURCES

- (i) **Based on consultation with the U. S. Fish and Wildlife Service, state whether the proposed action is likely to adversely affect endangered or threatened species or areas designated as a critical habitat, and if so, describe the effects.**

Applicant received a response dated April 10, 2014 from U.S. Fish and Wildlife Service stating *"Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed"*. (See Attachment 8)

- (ii) **State whether wildlife sanctuaries or refuges, National or State parks or forests will be affected, and describe any effects.**

Based upon Applicant's review of the area, the line is not within any wildlife sanctuaries or refuges, National or State parks or forests.

(9) WATER

- (i) **Based on consultation with State water quality officials, state whether the proposed action is consistent with applicable Federal, State or local water quality standards. Describe any inconsistencies.**

Applicant received a response dated May 2, 2014 from Environmental Coordinator Indiana Department of Natural Resources stating *"Formal approval by the Department of Natural*

Resources under the regulatory programs administered by the Division of Water is not required for this project. (See Attachment 9)

- (ii) **Based on consultation with the U. S. Army Corps of Engineers, state whether permits under Section 404 of the Clean Water Act (33 U.S.C. 1344) are required for the proposed action and whether any designated wetlands or 100-year flood plains will be affected. Describe the effects.**

Applicant has received a response from the U.S. Army Corps of Engineering, Louisville, KY dated May 6, 2014 stating that the proposed project will have "No environmental impact". (See Attachment 10)

Applicant is not aware of any designated wetlands or 100-year flood plains within the proposed project.

Upon receiving abandonment authority, removal of material will be accomplished by use of the right of way for access, along with existing public and private crossings, and no new access roads are contemplated. Applicant does not intend to disturb any of the underlying road-bed or perform any activities that would cause sedimentation or erosion of the soil, and do not anticipate any dredging or use of fill in the removal of the track material. The crossties and/or other debris will be transported away from the rail line and will not be discarded along the right of way nor be placed or left in streams or wetlands, or along the banks of such waterways. Also, during track removal, appropriate measures will be implemented to prevent or control spills from fuels, lubricants or any other pollutant materials from entering any waterways. Based upon this course of action, Applicant does not believe a permit under Section 404 of the Clean Water Act will be required.

- (iii) **State whether permits under Section 402 of the Clean Water Act (33 U.S.C. 1342) are required for the proposed action. (Applicants should contact the U. S. Environmental Protection Agency or the state environmental protection or equivalent agency if they are unsure whether such permits are required).**

Applicant has received a response from the U.S. EPA dated May 9, 2014 stating that the proposed project will have "No environmental impact". (See Attachment 11)

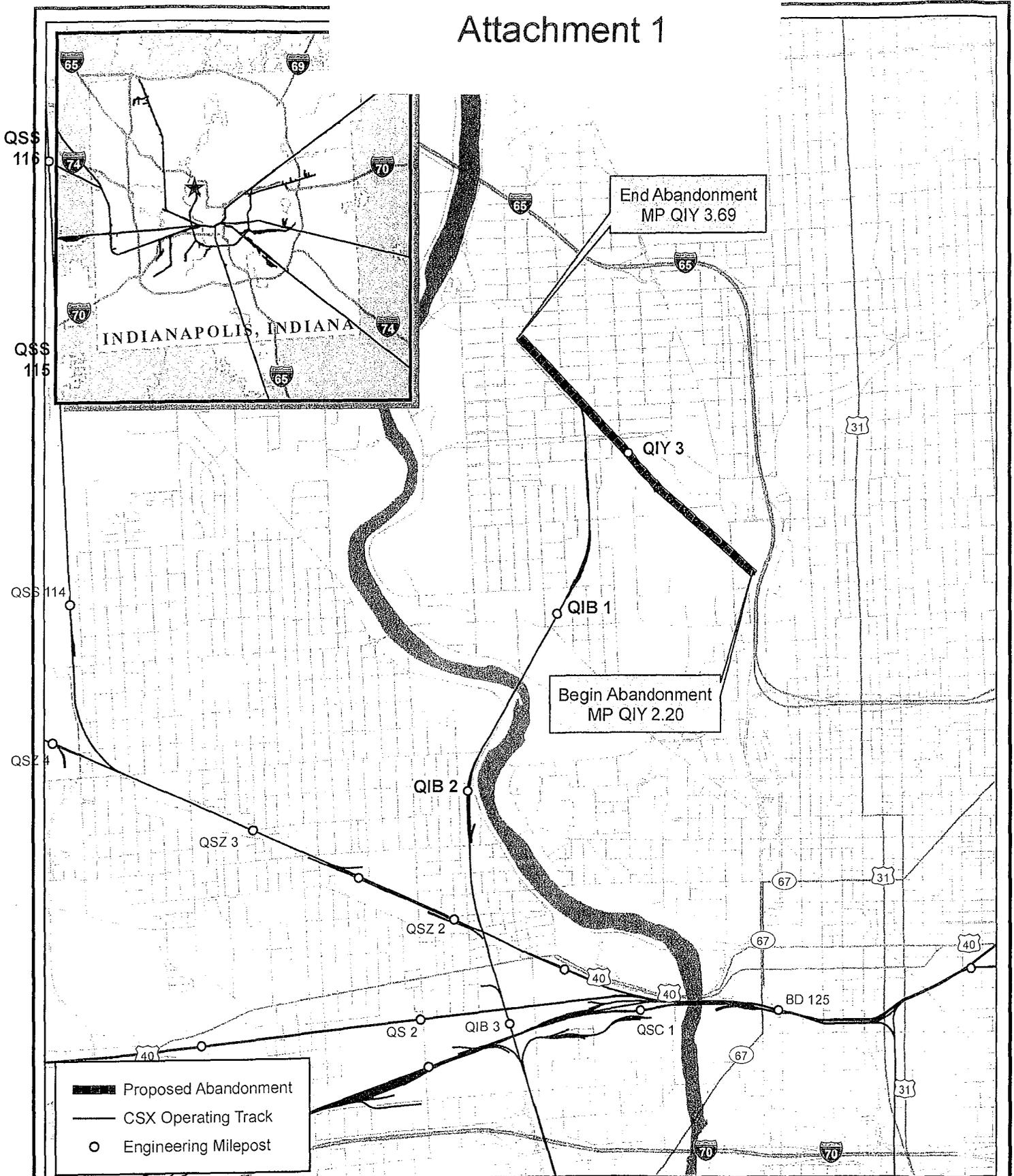
10. MITIGATION

Describe any actions that are proposed to mitigate adverse environmental impacts,

indicating why the proposed mitigation is appropriate.

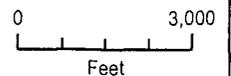
Applicant does not believe there will be any adverse environmental impacts in the project area as a result of this abandonment. However, Applicant will comply with reasonable State and Federal regulations and obtain any necessary permits required.

Attachment 1



CSX Real Property, Inc.

CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB 55 (Sub-No. 709X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana



Attachment 2

CSXT Abandonment External Distribution List Marion County Indianapolis, IN

Indiana Dept of Environmental Management
Indiana Government Center North, Room 1207
100 N. Senate Ave. MC 50-07
Indianapolis, IN 46204

U.S. EPA
Region 5
Ralph Metcalfe Federal Building
77 West Jackson Blvd
Chicago, IL 60604-3590

Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology
402 W. Washington ST., RM 274
Indianapolis, IN 46204

Indianapolis and Marion County
Department of Metropolitan Development
City-County Building
200 E. Washington Street
Suite 2042
Indianapolis, IN 46204

U.S. Army Corps of Engineers
Louisville District-Mazzoli Federal Bldg
600 Martin Luther King, Jr. Pl.
Louisville, KY 40202

U.S. Fish and Wildlife Service
Great Lakes-Big Rivers Region
620 South Walker Street
Bloomington, IN 47403-2121

Office of the Mayor
Mayor Gregory A. Ballard
2501 City-County Bldg.
200 E. Washington Street
Indianapolis, IN 46204

Division of Water
Indiana Department of Natural Resources
402 W. Washington ST., RM 264
Indianapolis, IN 46204

National Park Service
1849 C Street NW
Washington, DC 20240

Indiana Department of Transportation
Greenfield District
32 South Broadway
Greenfield, IN 46140
eastcentralin@indot.in.gov

USDA NRCS
Indiana State Office
6013 Lakeside Blvd
Indianapolis, Indiana 46278-2933



Attachment 3

500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

March 27, 2014

Indiana Department of Transportation
Greenfield District
32 South Broadway
Greenfield, IN 46140
eastcentralin@indot.in.gov

Dear Sir/Madam:

Please be advised that CSX Transportation, Inc. (CSXT) is considering to file with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana as depicted on the attached map. STB Docket number AB 55 (Sub-No. 709X).

The purpose of the proposed action is to abandon, discontinue service, salvage the track and improvements and subsequently lease or sell the property. There has been no traffic on the line for at least two (2) years.

This action requires Surface Transportation Board approval and Federal Regulations [49 C.F.R. 1105.7(3) (i)] require that CSXT develop a response to the following statement:

“Based on consultation with local and/or regional planning agencies and/or a review of the official planning documents prepared by such agencies, please state whether the proposed action is consistent with existing land use plans. Please describe any inconsistencies.”



500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

CSXT would appreciate your comments regarding current and future land use in relationship to the Line. As further information, CSXT will provide a preliminary environmental report in the coming weeks which will outline the scope of the proposed salvage activity and include any comments received from your office. This environmental report can be sent electronically by email or through the United States Postal Service (USPS). Please email Jo_Burroughs@csx.com if you wish to receive the report electronically. If we do not receive notification, it will be mailed via USPS.

Enclosed is a distribution list of all parties that have been copied on the proposed action. If you have any questions, please feel free to call or email me.

Sincerely,

Jo Ann Burroughs

Enclosures (2)



Jo Ann Burroughs
Manager Network Services

Attachment 4

500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

March 27, 2014

Indianapolis and Marion County
Department of Metropolitan Development
City-County Building
200 E. Washington Street
Suite 2042
Indianapolis, IN 46204

Dear Sir/Madam:

Please be advised that CSX Transportation, Inc. (CSXT) is considering to file with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana as depicted on the attached map. STB Docket number AB 55 (Sub-No. 709X).

The purpose of the proposed action is to abandon, discontinue service, salvage the track and improvements and subsequently lease or sell the property. There has been no traffic on the line for at least two (2) years.

This action requires Surface Transportation Board approval and Federal Regulations [49 C.F.R. 1105.7(3) (i)] require that CSXT develop a response to the following statement:

“Based on consultation with local and/or regional planning agencies and/or a review of the official planning documents prepared by such agencies, please state whether the proposed action is consistent with existing land use plans. Please describe any inconsistencies.”

CSXT would appreciate your comments regarding current and future land use in relationship to the Line. As further information, CSXT will provide a preliminary environmental report in the



500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

coming weeks which will outline the scope of the proposed salvage activity and include any comments received from your office. This environmental report can be sent electronically by email or through the United States Postal Service (USPS). Please email Jo_Burroughs@csx.com if you wish to receive the report electronically. If we do not receive notification, it will be mailed via USPS.

Enclosed is a distribution list of all parties that have been copied on the proposed action. If you have any questions, please feel free to call or email me.

Sincerely,

Jo Ann Burroughs

Enclosures (2)



Jo Ann Burroughs
Manager Network Services

Attachment 5

500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
Mail: Jo_Burroughs@csx.com

March 27, 2014

Office of the Mayor
Mayor Gregory A. Ballard
2501 City-County Building
200 E. Washington Street
Indianapolis, IN 46204

Dear Mayor Ballard;

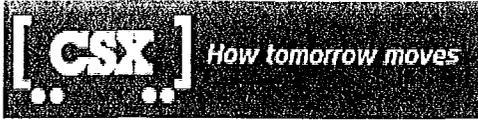
Please be advised that CSX Transportation, Inc. (CSXT) is considering to file with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana as depicted on the attached map. STB Docket number AB 55 (Sub-No. 709X).

The purpose of the proposed action is to abandon, discontinue service, salvage the track and improvements and subsequently lease or sell the property. There has been no traffic on the line for at least two (2) years.

This action requires Surface Transportation Board approval and Federal Regulations [49 C.F.R. 1105.7(3) (i)] require that CSXT develop a response to the following statement:

“Based on consultation with local and/or regional planning agencies and/or a review of the official planning documents prepared by such agencies, please state whether the proposed action is consistent with existing land use plans. Please describe any inconsistencies.”

CSXT would appreciate your comments regarding current and future land use in relationship to the Line. As further information, CSXT will provide a preliminary environmental report in the coming weeks which will outline the scope of the proposed salvage activity and include any comments received from your office. This environmental report can be sent electronically by



500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

email or through the United States Postal Service (USPS). Please email Jo_Burroughs@csx.com if you wish to receive the report electronically. If we do not receive notification, it will be mailed via USPS.

Enclosed is a distribution list of all parties that have been copied on the proposed action. If you have any questions, please feel free to call or email me.

Sincerely,

Jo Ann Burroughs

Enclosures (2)



Attachment 6

Natural Resources Conservation Service
Indiana State Office
6013 Lakeside Boulevard
Indianapolis, IN 46278
317-290-3200

April 1, 2014

Jo Ann Burroughs
Manager Network Services
CSX Transportation, Inc.
500 Water Street – J315
Jacksonville, Florida 32202

Dear Ms. Burroughs:

The proposed project regarding the action to abandon, discontinue service, salvage the track from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69, in the City of Indianapolis, Marion County, Indiana, as referred to in your letter received March 31, 2014, will not cause a conversion of prime farmland.

If you need additional information, please contact Rick Neilson at 317-295-5875.

Sincerely,

JANE E. HARDISTY
State Conservationist





Jo Ann Burroughs
Manager Network Services

Attachment 7

500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
ail: Jo_Burroughs@csx.com

March 27, 2014

Division of Water
Indiana Department of Natural Resources
402 W. Washington ST., RM 264
Indianapolis, IN 46204

Dear Sir/Madam:

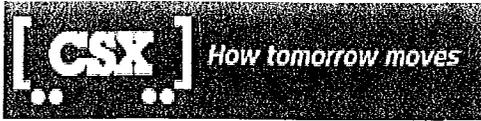
Please be advised that CSX Transportation, Inc. (CSXT) is considering to file with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana as depicted on the attached map. STB Docket number AB 55 (Sub-No. 709X).

The purpose of the proposed action is to abandon, discontinue service, salvage the track and improvements and subsequently lease or sell the property. There has been no traffic on the line for at least two (2) years.

This action requires Surface Transportation Board approval and Federal Regulations 49 C.F.R. 1105.7 (9) (i) and (iii) require that CSXT develop a response to the following statements:

(i) *“Based on consultation with State Water quality officials, state whether the proposed action is consistent with applicable Federal, State or local water quality standards. Describe any inconsistencies.”*

(iii) *“State whether permits under Section 402 of the Clean Water Act 33 U.S.C. 1342 are required for the proposed action.”*



500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

There are no refueling or maintenance areas within the project area. The removal of CSXT's rail material will be accomplished by use of the right of way for access, along with existing public and private crossings, and no new access roads are contemplated. CSXT does not intend to disturb any of the underlying roadbeds and do not anticipate any dredging or use of fill in the removal of the track material. The crossties and/or other debris will be transported away from the rail line and will not be discarded along the right of way, nor be placed or left in streams or wetlands, or along the banks of such waterways. During track removal, appropriate measures will be implemented to prevent or control spills from fuels, lubricants or any other pollutant materials from entering any watercourses.

Based upon the above described actions, CSXT would appreciate receiving your concurrence with its position that the proposed project is consistent with applicable Federal, State and local water quality standards, and that no permits under Section 402 are required at this time. As further information, CSXT will provide a preliminary environmental report in the coming weeks which will outline the scope of the proposed salvage activity and include any comments received from your office. This environmental report can be sent electronically by email or through the United States Postal Service (USPS). Please email Jo_Burroughs@csx.com if you wish to receive the report electronically. If we do not receive notification, it will be mailed via USPS.

Enclosed is a distribution list of all parties that have been copied on the proposed action. If you have any questions, please feel free to call or email me.

Sincerely,

Jo Ann Burroughs
Enclosures (2)

Attachment 8

Burroughs, JoAnn

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Thursday, April 10, 2014 1:46 PM
To: Burroughs, JoAnn
Subject: CSX abandonment Indianapolis, IN Sub-No. 709X

Categories: Follow up

Dear Ms. Burroughs,

This responds to your recent letter, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed. This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change such that fish and wildlife habitat may be affected, please re-coordinate with our office as soon as possible. If you have any questions about our recommendations, please call (812)334-4261.

Sincerely,

Robin McWilliams Munson

Robin McWilliams Munson

U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, Indiana 46403
812-334-4261 Fax: 812-334-4273

Schedule

Monday, Tuesday - 7:30a-3:00p

DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-17546

Request Received: April 1, 2014

Requestor: CSX Transportation Inc
Jo Burroughs
500 Water Street - J-315
Jacksonville, FL 32202

Project: Abandonment of 1.49 miles of CSXT rail line from Milepost QIY 2.20 to QIY 3.69, Indianapolis; STB Docket No. AB 55 (Sub-No. 709X)

County/Site info: Marion

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

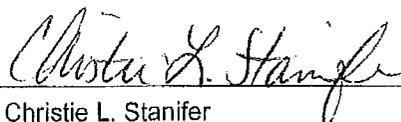
Natural Heritage Database: The Natural Heritage Program's data have been checked. The federally and state endangered snuffbox (*Epioblasma triquetra*) has been recorded in Fall Creek within 1/2 mile north of the east end of the project area.

Fish & Wildlife Comments: The snuffbox is no longer found live in Fall Creek; therefore, we do not foresee any impacts to this listed mussel species as a result of the project.

The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion.
2. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
3. Seed and protect all disturbed streambanks and slopes that are 3:1 or steeper with erosion control blankets (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: May 2, 2014

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife



Attachment 10

500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

CSXT Environmental Report Response Form

RE: CSX Transportation, Inc. - Proposed Abandonment - STB Docket No. AB-55 (Sub No. 709X)

CSX Transportation, Inc. (CSXT) is considering filing with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana. STB Docket number AB 55 (Sub-No. 709X).

Agency response (check one):

No Environmental Impact

Environmental Impact as follows (or attached):

Date: 5/6/14

Signature: Greg McKay

Name: GREG MCKAY 502-315-6685

Title: CHIEF, NORTH SECTION, REGULATORY BRANCH
U.S. ARMY CORPS OF ENGINEERS, LOUISVILLE DISTRICT



Jo Ann Burroughs
Manager Network Services

Attachment 11

Water Street - J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
j_burroughs@csx.com

CSXT Environmental Report Response Form

RE: CSX Transportation, Inc. - Proposed Abandonment - STB Docket No. AB-55 (Sub No. 709X)

CSX Transportation, Inc. (CSXT) is considering filing with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana. STB Docket number AB 55 (Sub-No. 709X).

Agency response (check one):

No Environmental Impact

Environmental Impact as follows (or attached):

Date: 5/9/2014
Signature:
Name: KENNETH A. WESTLAKE
Title: CHIEF, NEPA IMPLEMENTATION SECTION

Burroughs, JoAnn

From: Simon Monroe <simon.monroe@noaa.gov>
Sent: Wednesday, April 09, 2014 2:12 PM
To: Burroughs, JoAnn
Cc: Surface Transportation Board; Lucy Hall; Simon Monroe
Subject: NGS Response, STB Docket AB-55 (SUB NO. 709X)

Thank you for sharing your railroad abandonment environmental report for
INDIANAPOLIS, Marion County, INDIANA.

Approximately 00 geodetic survey marks may be located in the area described. These marks may or may not be disturbed.

If marks will be disturbed by the abandonment, [THE RAILROAD] shall
consult with the National Geodetic Survey (NGS) at least 90 days prior to
beginning salvage activities that will disturb, or destroy any geodetic station
marks are described on the attached file. Additional advice is provided at
<http://geodesy.noaa.gov/marks/railroads/>

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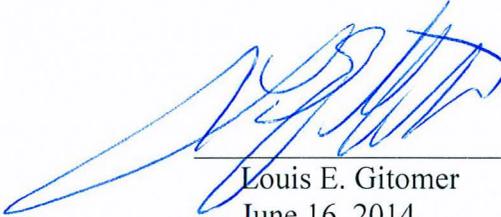
No Stations Found.

EXHIBIT E- HISTORIC REPORT

HISTORIC REPORT CERTIFICATE OF SERVICE

Pursuant to the requirements of 49 C.F.R. §1105.8(c), the undersigned hereby certifies that a copy of the Historic Report in Docket No. AB-55 (Sub-No. 709X) was mailed via first class mail on April 22, 2014, to the following party:

Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room 274
Indianapolis, IN 46204



Louis E. Gitomer
June 16, 2014

HISTORIC REPORT

**CSX TRANSPORTATION, INC.
INDIANAPOLIS, MARION COUNTY, INDIANA
DOCKET AB-55 (SUB-NO. 709X)**

1105.7(e)(1)

PROPOSED ACTION AND ALTERNATIVES. Describe the proposed action, including commodities transported, the planned disposition (if any) of any rail line and other structures that may be involved, and any possible changes in current operations or maintenance practices. Also describe any reasonable alternatives to the proposed action. Include a readable detailed map and drawings clearly delineating the project.

CSXT proposes to abandon approximately 1.49 miles of its rail line between Railroad Milepost QIY 2.20 and Railroad Milepost QIY 3.69 known as the North Street Industrial Track, in Indianapolis, Marion County, Indiana.

The above referenced 1.49-mile line has not generated any originating or terminating traffic in over two (2) years.

Abandonment of this line will result in the removal of the rail, crossties, and possibly the upper layer of ballast. CSXT does not intend to disturb any sub grade or sub grade structures. Finally, the operations and maintenance of this line will cease.

The only alternative would be not to abandon and to pass the opportunity costs of retaining the line to all other CSXT customers. This would not be a prudent utilization of carrier resources.

A map which delineates the proposed project is attached. (See Attachment 1)

1105.8(d)

- (1) A U.S.G.S. topographic map (or an alternate map drawn to scale and sufficiently detailed to show buildings and other structures in the vicinity of the proposed action) showing the location of the proposed action, and the locations and approximate dimensions of railroad structures that are 50**

years old or older and are part of the proposed action.

Attached is a copy of the Indianapolis West quadrangle topographic map prepared by the U. S. Department of Interior Geological Survey.

The line to be abandoned has been identified by a heavy red line with black dashes. (See Attachment 2)

There are two (2) CSXT-owned structures that are 50 years old or older that may be eligible for listing in the National Register that are part of the proposed action. The first bridge is located at Milepost Marker QIY 2.79 and the second bridge is located at Railroad Milepost Marker QIY 3.10.

- (2) A written description of the right of way (including approximate widths, to the extent known), and the topography and urban and/or rural characteristic of the surrounding area:**

The right of way width along this right of way is approximately 32 to 45 feet from the centerline of track. The line traverses Indianapolis industrial and residential areas near the central canal that runs in a southeasterly direction.

- (3) Good quality photographs of railroad structures on the property that are 50 years old or older and of the immediately surrounding area:**

Digital Photo copies of the bridges located at QIY 2.79 and QIY 3.10 are attached. (See Attachments 3 &4).

- (4) The date(s) of construction of the structure(s), and the date(s) and extent of any major alterations, to the extent such information is known:**

Bridge QIY 2.79 also known as the Fall Creek Bridge No. 498 for the Cleveland Cincinnati Chicago & St. Louis Railway (CCC & StL.), Chicago Division appears to be built by the Massillon Bridge Company from plans submitted on July 1, 1895 and revised July 27, 1895. The bridge plans also state there were major modifications for a bridge extension on plans submitted January 1915. (See Attachment 5)

Bridge QIY 3.10 also known as the Broad Cut Canal No. 499 for the

CCC & StL., Chicago Division, appears to be built in 1930 from plans dated January 19, 1924 and December 16, 1929. It is a steel & timber trestle with Bethlehem girder beams. (See Attachment 6)

(5) A brief narrative history of carrier operations in the area, and an explanation of what, if any, changes are contemplated as a result of the proposed action:

This line was acquired by the Cleveland, Cincinnati, Chicago and St. Louis Railway (CCC&StL.), also known as the Big Four Railroad, in the late 19th century. The CCC&StL was acquired in 1906 by the New York Central Railroad. The former lines of CCC&StL were incorporated into Penn Central in 1968 with the merger of New York Central Railroad and the Pennsylvania Railroad. Penn Central declared bankruptcy in 1970 and the line was included in the lines of the newly formed Conrail. In 1999, the line was acquired by CSXT when Conrail was divided between CSXT and Norfolk Southern.

Upon receiving abandonment authority, Applicant's operations and maintenance over this line will cease.

(6) A brief summary of documents in the carrier's possession, such as engineering drawings, that might be useful in documenting a structure that is found to be historic:

Bridge plans in Attachment 5 and 6 are also available as a PDF upon request.

(7) An opinion (based on readily available information in the railroad's possession) as to whether the site and/or structures meet the criteria for listing on the National Register of Historic Places (36 C.F.R. 60.4), and whether there is a likelihood of archeological resources or any other previously unknown historic properties in the project area, and the basis for these opinions (including any consultations with the State Historic Preservation Office, local historical societies or universities):

A review of our records indicates there are two (2) CSXT-owned structures over 50 years old on this line segment that may be eligible for listing in the National Register.

We do not know of any significance or uniqueness to this structure that would warrant your consideration, since to our knowledge, it is not associated with any event that has made a contribution to the

broad patterns of history; or were not associated with lives of persons significant to our past; do not embody the distinctive characteristics of a type, period or method of construction; and do not represent a significant and distinguishable entity whose components may lack individual distinction and have not or may not be likely to yield information important in prehistory or history.

We do not know of any archeological resources or any other previously unknown historic properties in the project area.

- (8) A description (based on readily available information in the railroad's possession) of any known prior subsurface ground disturbance or fill, environmental conditions (naturally occurring or man-made) that might affect the archeological recovery of resources (such as swampy conditions or the presence of toxic wastes), and the surrounding terrain.**

The line was disturbed during construction by cuts and fill and any archeological resources that may have been located in the proposed project area would have been affected at that time versus during the proposed salvage operations associated with rail removal. Our records do not indicate that any swampy conditions exist, or that any hazardous material spills have occurred within the project area.



500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

April 22, 2014

Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology
402 W. Washington ST., RM 274
Indianapolis, IN 46204

Dear Sir/Madam;

Please be advised that CSX Transportation, Inc. (CSXT) is considering to file with the Surface Transportation Board (STB) a Notice of Exemption seeking authority to abandon common carrier obligation in the Northern Region, Great Lakes Division, Indianapolis Terminal Subdivision, from Railroad Milepost QIY 2.20 to the end of track Railroad Milepost QIY 3.69 also known as the North Street Industrial Track. The distance is approximately 1.49 miles in Indianapolis, Marion County, Indiana as depicted on the attached map. STB Docket number AB 55 (Sub-No. 709X).

The purpose of the proposed action is to abandon, discontinue service, salvage the track and improvements and subsequently lease or sell the property. There has been no traffic on the line for at least two (2) years.

In connection with rail lines that are to become the subject of applications for authority to abandon, Federal Regulations at 49 CFR 1105.8(d), require that a Historic Report be submitted to the State Historic Preservation Officer prior to filing with the Surface Transportation Board. In accordance with those Regulations, I am attaching a Historic Report covering the above-proposed abandonment.



500 Water Street – J-315
Jacksonville, FL 32202
Phone: (904) 359-1247
E-Mail: Jo_Burroughs@csx.com

Jo Ann Burroughs
Manager Network Services

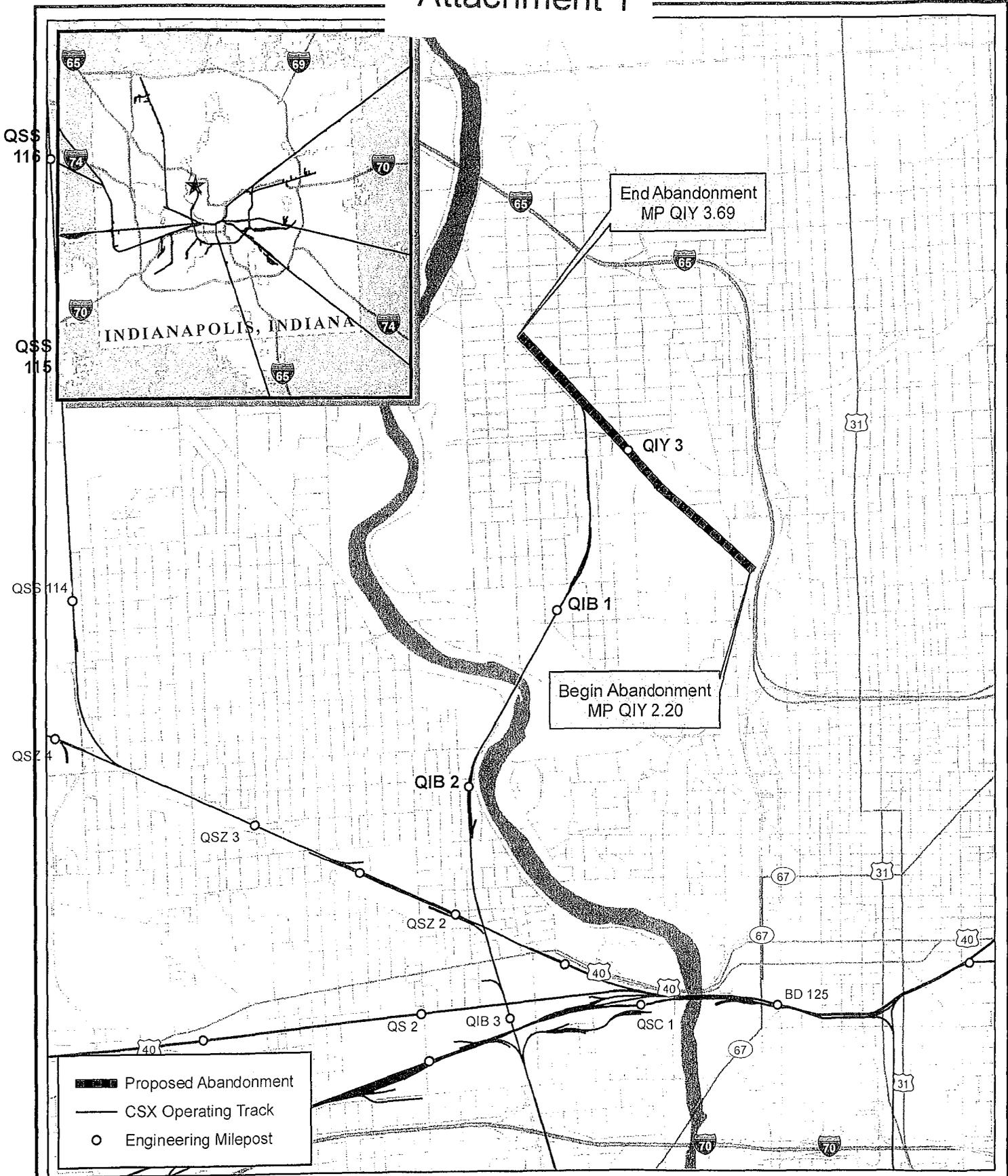
We would appreciate receiving a letter from your office confirming that this project will have no impact upon cultural resources.

Enclosed is a distribution list of all parties that have been copied on the proposed action. If you have any questions, please feel free to call or email me.

Sincerely,

Jo Ann Burroughs
Enclosures (2)

Attachment 1



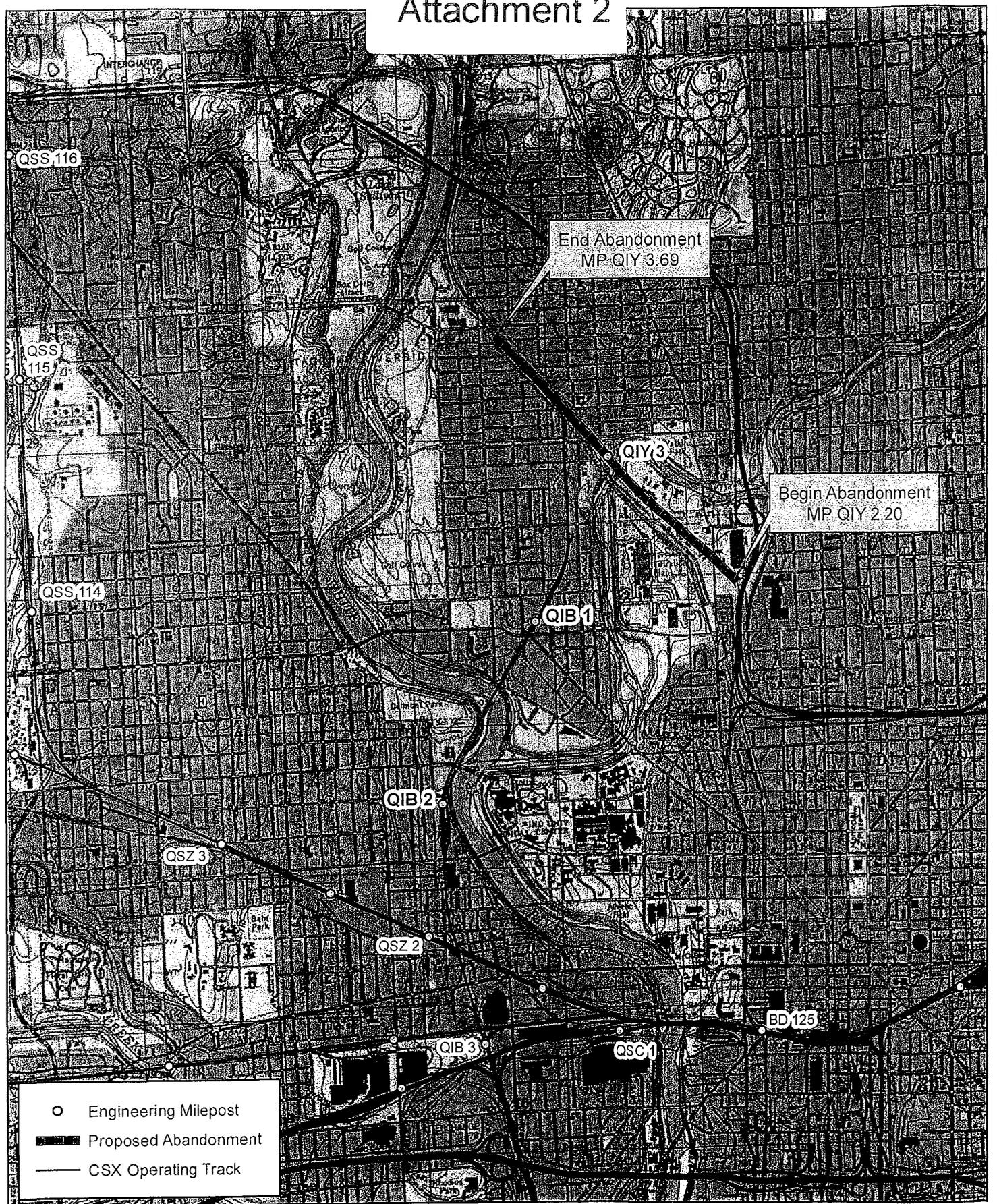
CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB 55 (Sub-No. 709X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana



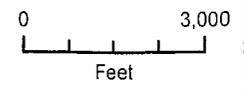
Feet



Attachment 2



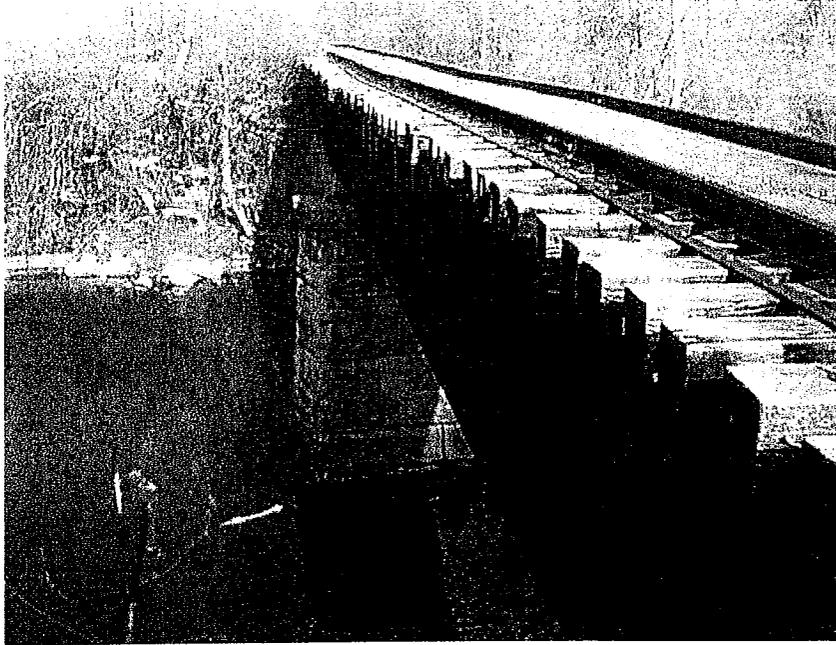
CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB55 (Sub No. 709 X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana



Attachment 3
Bridge 498
Milepost QIY 2.79



Attachment 3
Bridge 498
Milepost QIY 2.79



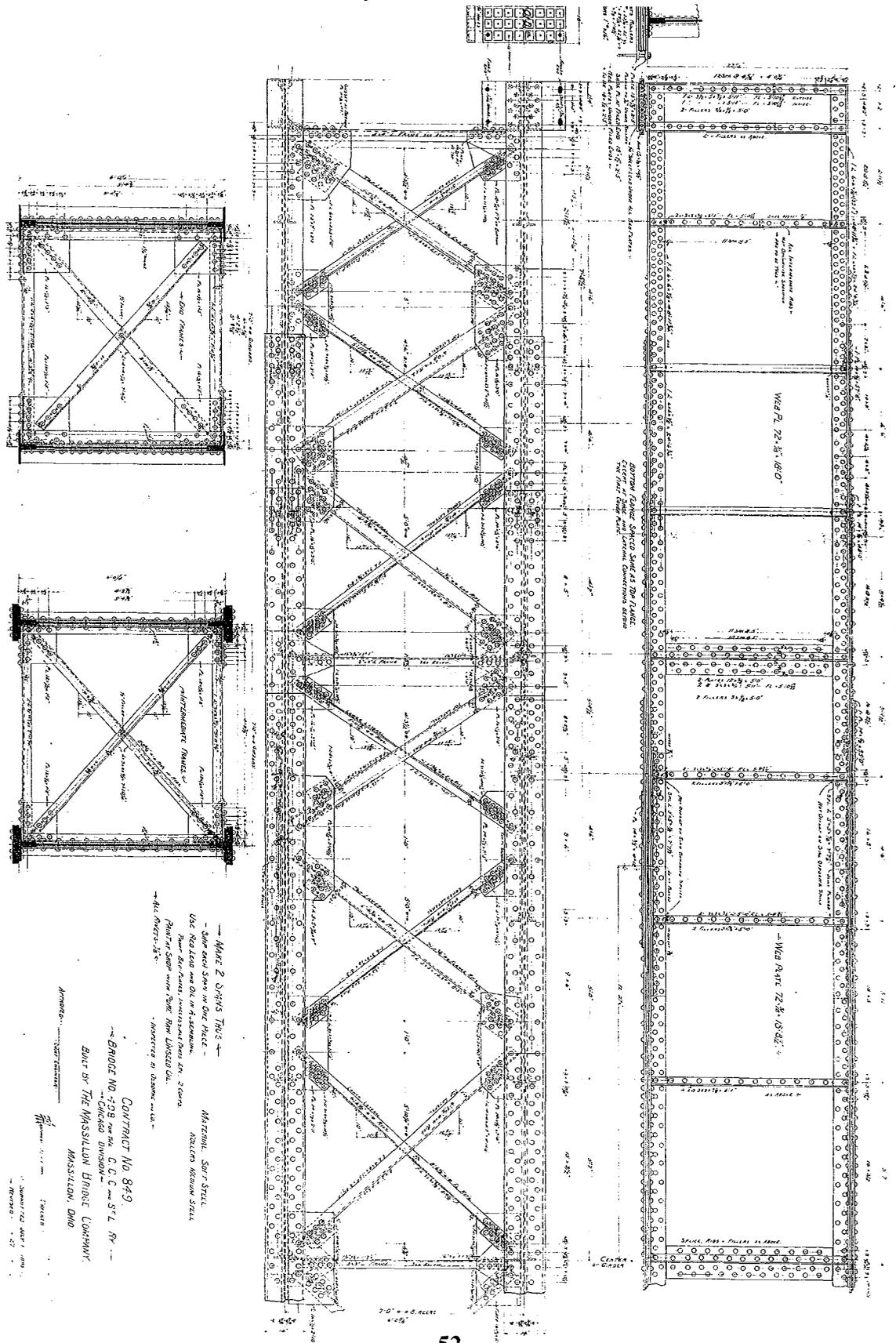
Attachment 4
Bridge 499
Milepost QIY 3.10

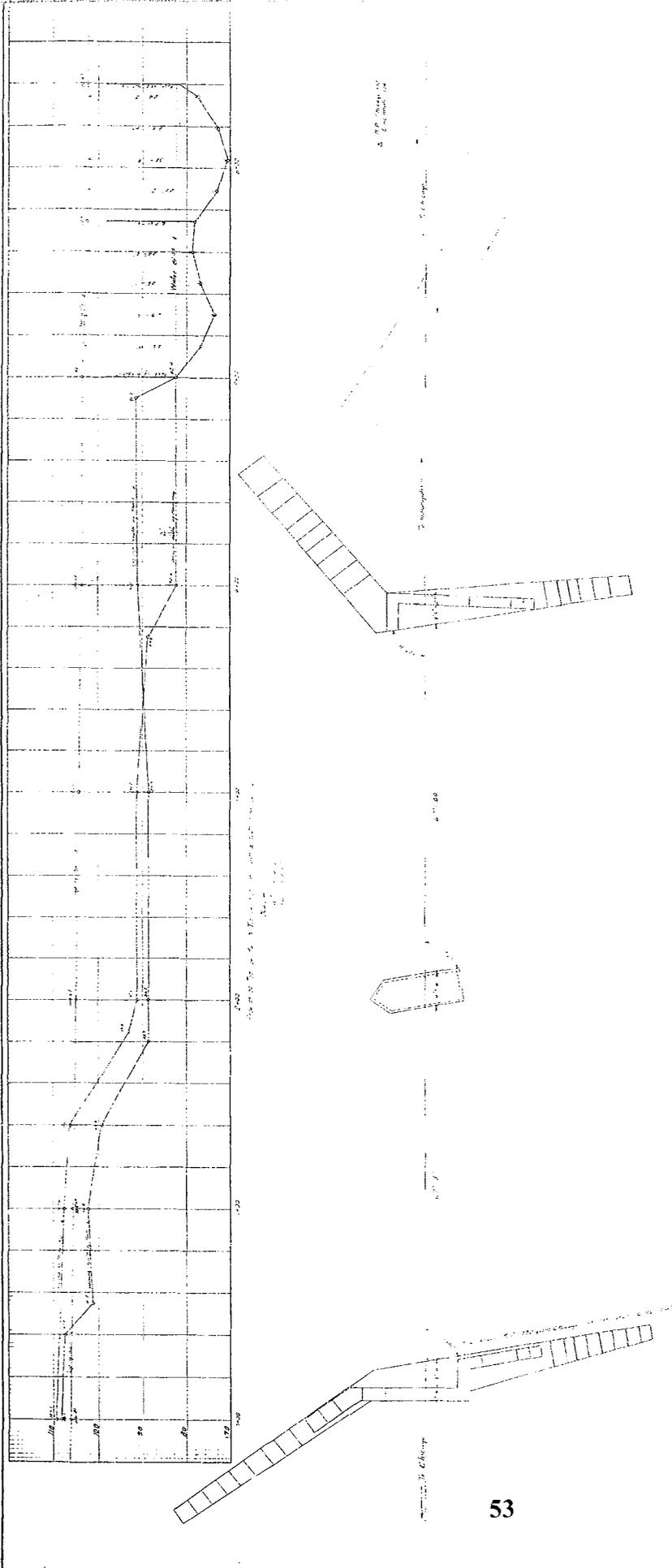


Attachment 4
Bridge 499
Milepost QIY 3.10

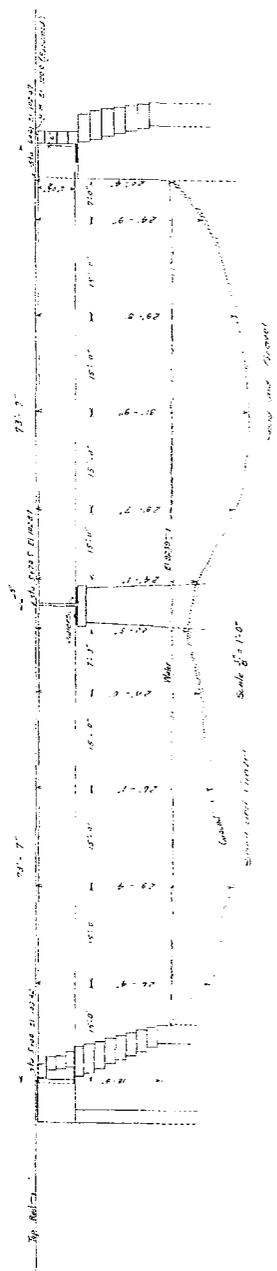


Attachment 5 Bridge 498 Milepost QIY 2.79





Note: Following information was prepared by Engineers who conducted the inspection which was made with the following notes: "The building is a single story structure with a concrete floor and a concrete ceiling. The building is located on a lot which is 100 feet wide and 100 feet deep. The building is situated on the corner of the lot. The building is situated on the corner of the lot. The building is situated on the corner of the lot."

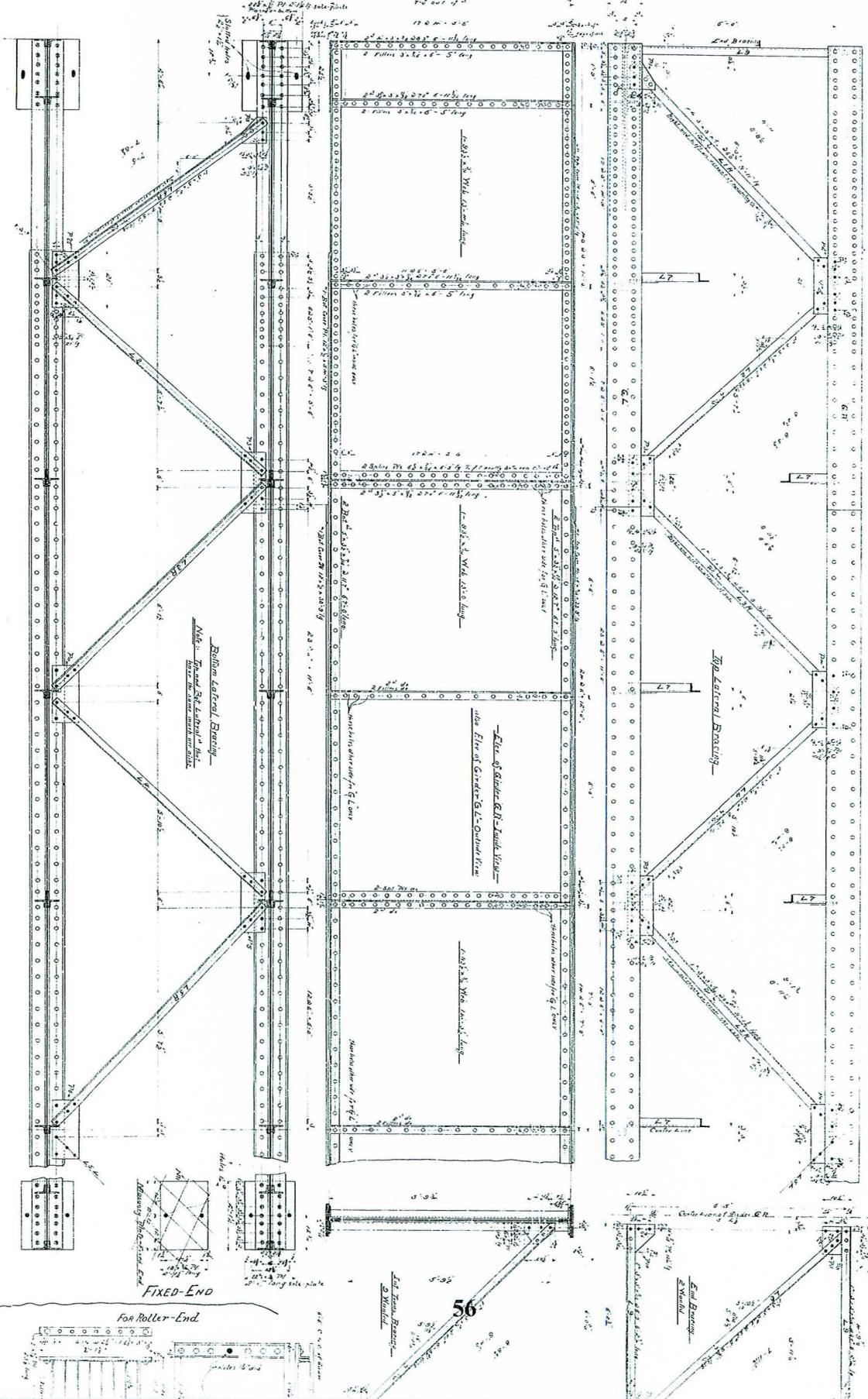


PIE-2446574

245716

Roller-End

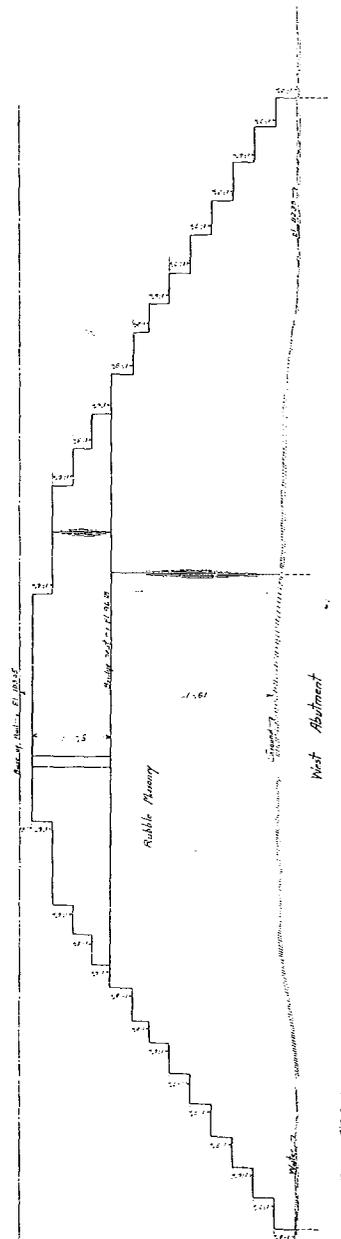
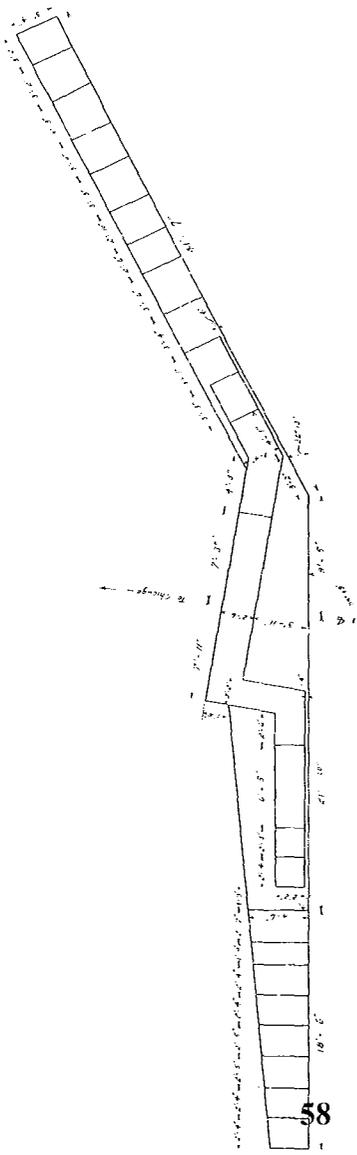
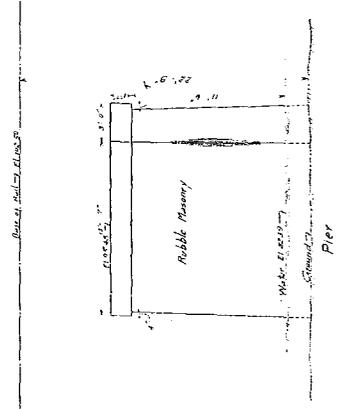
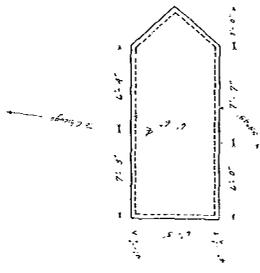
End of Drive



F 1

0770 and 6 drawings

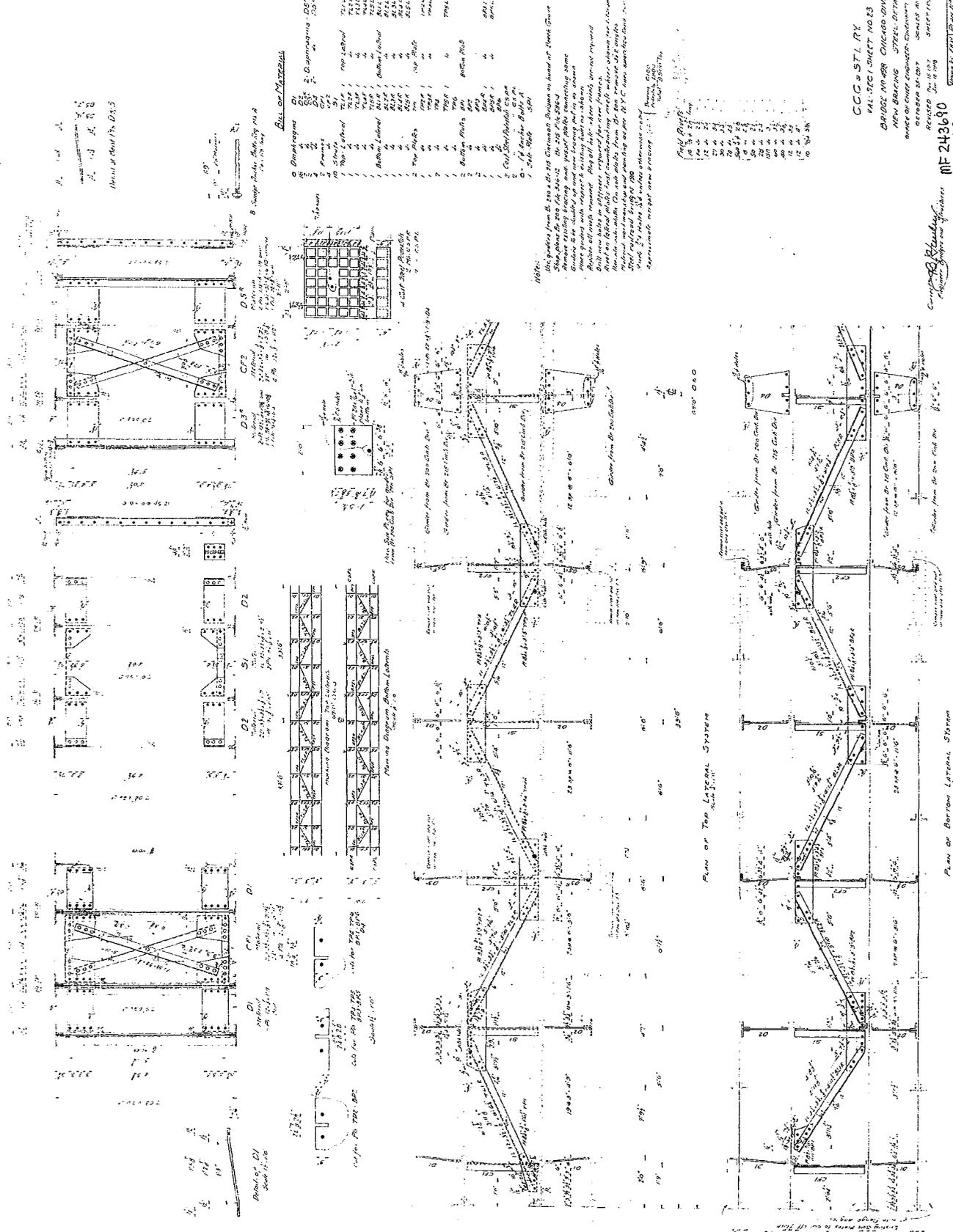
56



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11/26/78

F1

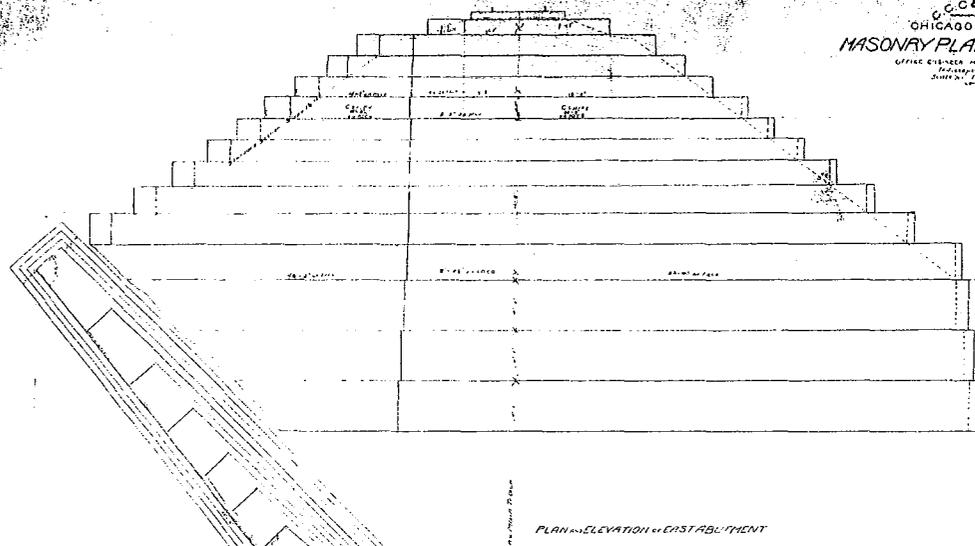


LIST OF MATERIALS

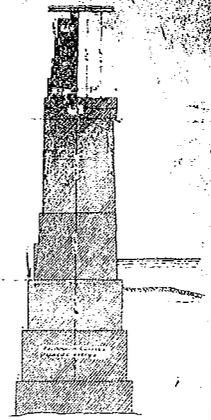
Quantity	Description	Material	Notes
1	Top Chord	10" x 12" S	
1	Bottom Chord	10" x 12" S	
1	Web Members	4" x 6" S	
1	Diagonal Members	4" x 6" S	
1	Vertical Members	4" x 6" S	
1	Horizontal Members	4" x 6" S	
1	Top Flange	1/2" x 12" PL	
1	Bottom Flange	1/2" x 12" PL	
1	End Connections	1/2" x 12" PL	
1	Stiffeners	1/2" x 12" PL	
1	Bracing	1/2" x 12" PL	
1	Deck	1/2" x 12" PL	
1	Substructure	1/2" x 12" PL	

CCC & S.T.L.R.Y.
 VAL. SPEC. SHEET NO. 23
 BRIDGE NO. 498 CHICAGO DIVISION
 NEW BRACKEN STREET BRIDGES
 BRIDGE OVER CHICAGO RIVER, CHICAGO, ILL.
 REVISED 10/13/78
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 APPROVED BY: [Name]

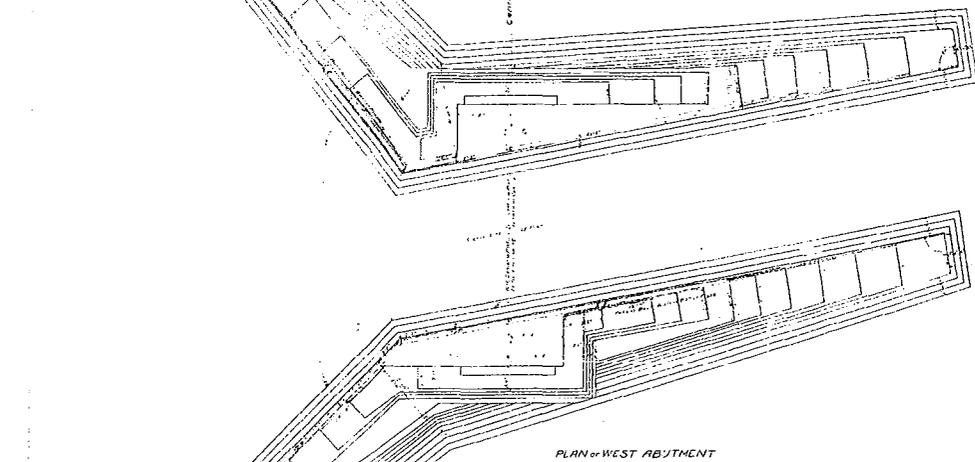
C. C. & B. L. A. E.
 CHICAGO DIVISION
MASONRY PLAN, EDGE 298
 OFFICE ENGINEER MAINTENANCE DIVISION
 PROJECT NO. 1015-22
 DATE 11-1-22



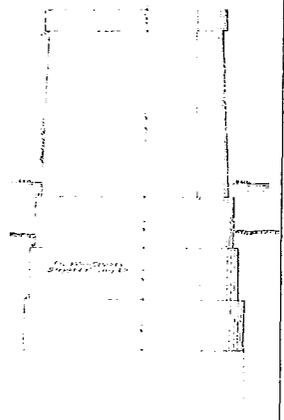
PLAN & ELEVATION OF EAST ABUTMENT



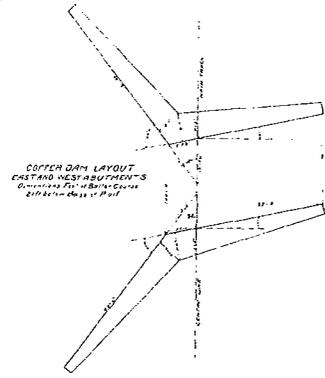
ABUTMENTS
 Section on Center Line of Main Truss



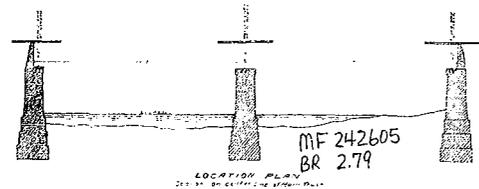
PLAN OF WEST ABUTMENT



PLAN & ELEVATION OF PIER



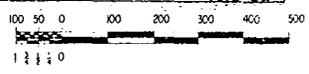
CENTER DAM LAYOUT
 EAST AND WEST ABUTMENTS
 CENTER LINE OF MAIN TRUSS
 SECTION ON CENTER LINE OF MAIN TRUSS

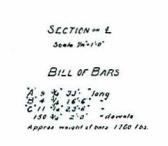
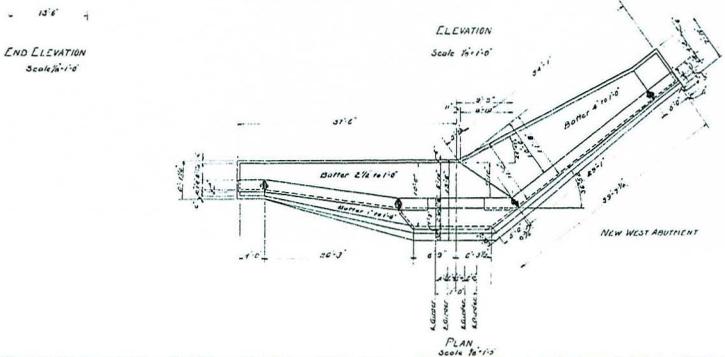
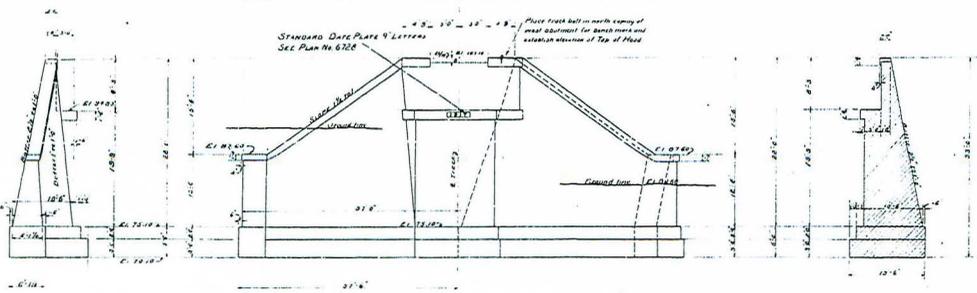
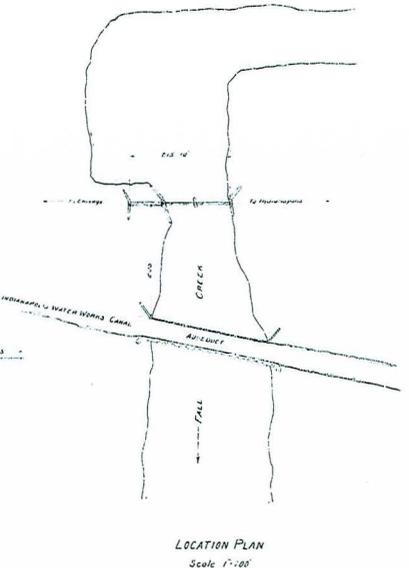
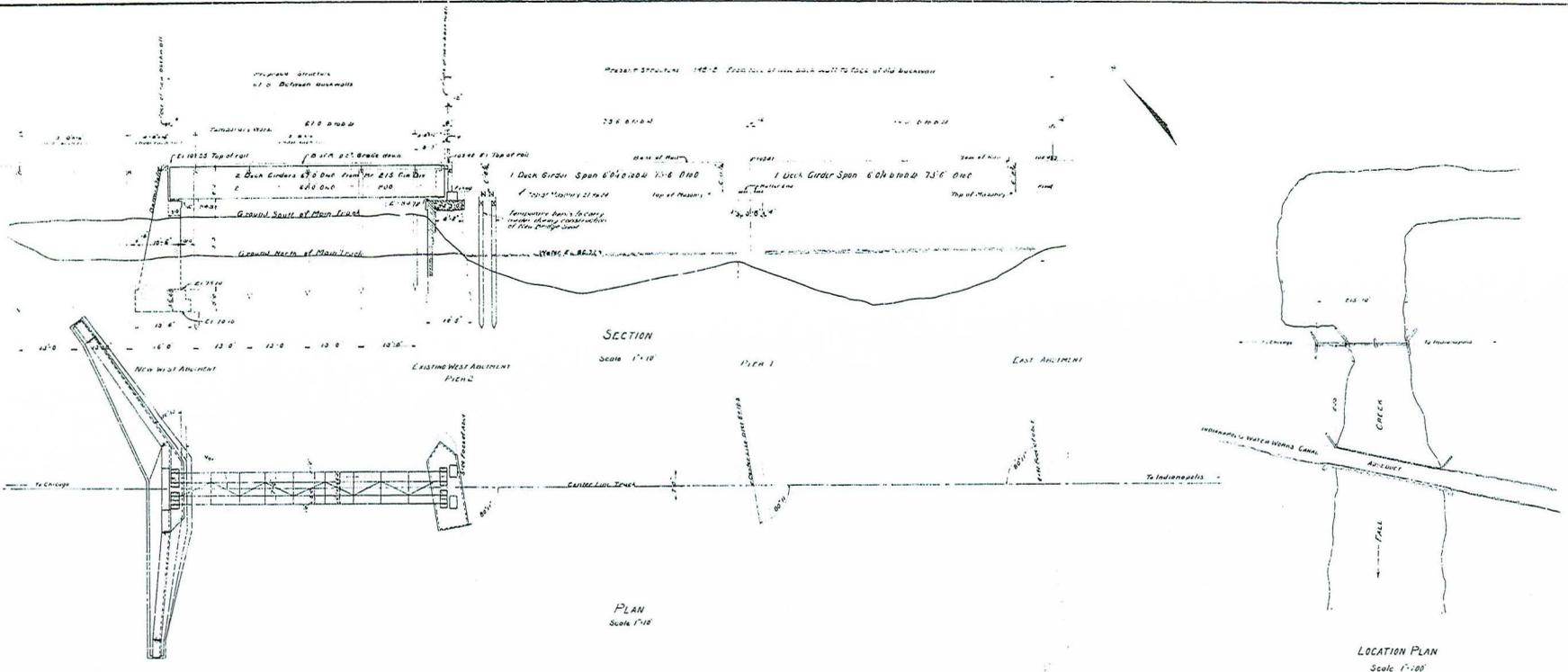


LOCATION PLAN
 SECTION ON CENTER LINE OF MAIN TRUSS

MF 242605
 BR 2.79

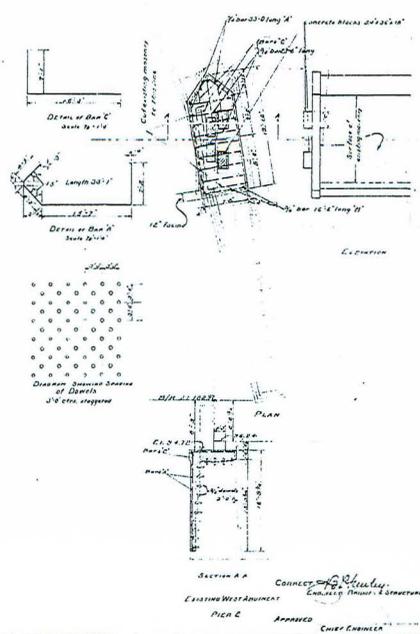
9217
 1015-22





APPROXIMATE QUANTITIES

CLASS	CON	ABUTMENT	PIER	TOTAL
FOOTING	146			146
NEAT	331	33		364
				510



FINAL REPORT ON MASONRY AS BUILT
STATION AND PLUS AT CENTER OF BRIDGE
C. C. AUTHORITY AT ... BUILT BY ... WORK COMPLETED ...
WORK STARTED ...

DESCRIPTION	ACTUAL QUANTITIES		PIER NO.	TOTAL QUANTITIES
	WEST	EAST		
Grading Cu Yards				
Dry Earth Estimated Cu Yds				
Dry Rock				
Foundation Piling Linear Ft				
Cofferdam Timber Ft. B.F.				
Steel Sheet Piling Vertical				
Manhole				
Manufacturing Draw. Lbs				
Class C Concrete Cu Yds				
Class D				
Shoe Added to Class D Concrete				
Water Proofing Sq. Ft.				
Elevation 4th				
Top of Ceiling				
Footings				
Bottom of				
Character of Foundation				
Bench mark used in construction, Location				See Level Defn.
Bench mark established on structure, Elevation				See Level Defn.
Remarks				

I hereby certify that the above information is correct and that this print
gives elevations from the plan as correctly shown
DATE

(Place) DATE

The above information and changes shown on this print have been re-
checked on the original drawing this date:
Checked by **ME 243474** Draftsman
DR 274

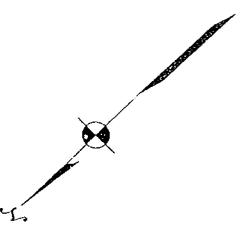
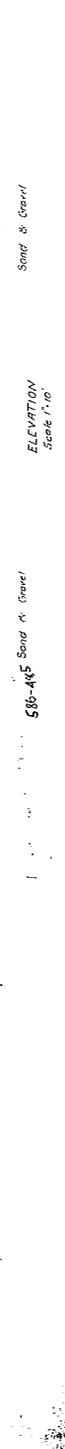
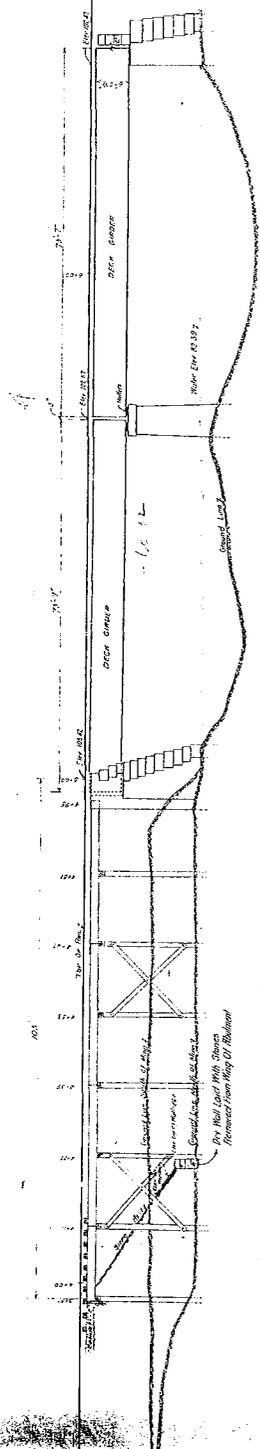
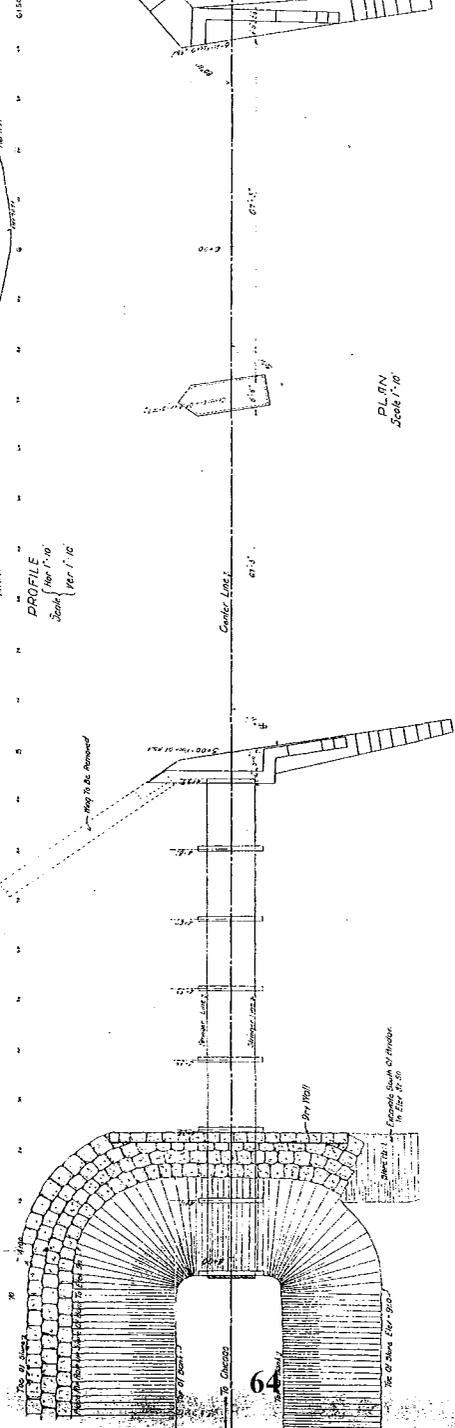
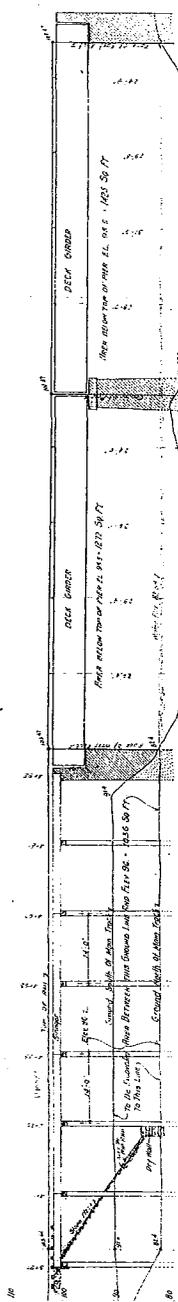
CLEVELAND, CINCINNATI, CHICAGO AND ST. LOUIS RY CO
BRIDGE NO. 496 CHICAGO DIVISION
VAL SEC SHEET
NEW WEST ABUTMENT AND ALTERATIONS TO EXISTING WEST ABUTMENT
MASONRY PLAN

SEPTEMBER 5, 1917 OFFICE OF CHIEF ENGINEER, CLEVELAND, OHIO
REVISED OCT. 12, 1917 SCALED AS SHOWN
1013-81 573-205



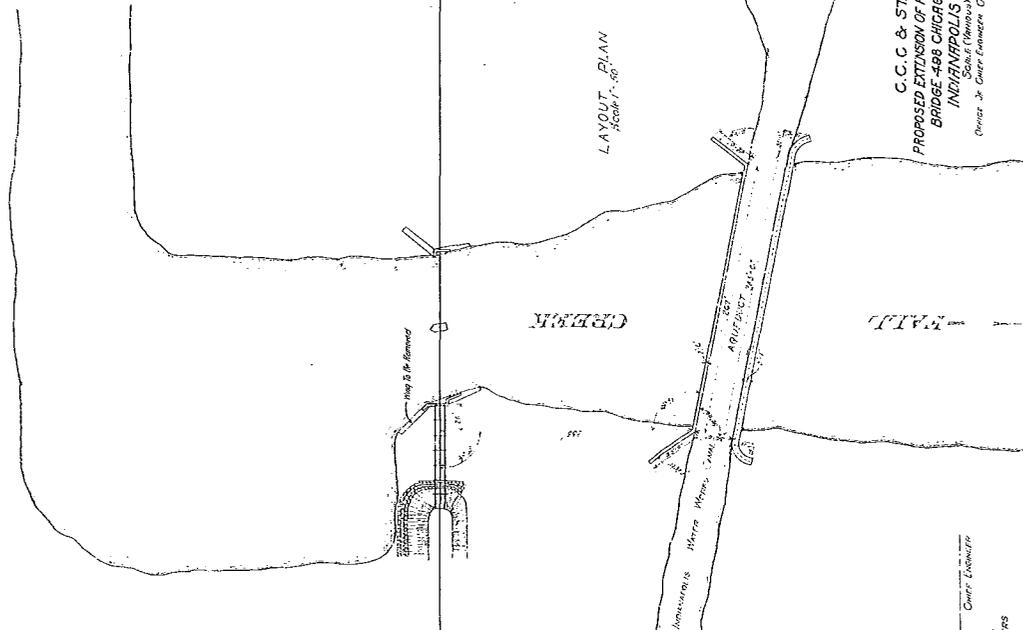
Sheet No. 131
 Division Chicago
 Date Recd. Plans 3-15-13
 By Messillon Br Co.
 Date 1835
 Material S&P Steel
 Thickness $\frac{7}{8}$
 End Stems $5 \times 3\frac{1}{2} \times \frac{7}{8}$
 Inter. Stems $3 \times 3 \times \frac{7}{8}$
 Web $72 \times \frac{7}{8}$
 Flanges $6 \times 6 \times \frac{7}{8}$ L 4
 I.C.P. $14 \times 5\frac{1}{2} \times 35$ C.B. $14 \times 5\frac{1}{2} \times 40$
 I.C.P. $14 \times 5\frac{1}{2} \times 57-8$
 L. of Section Av. Fl. (Web not included)
 Impact 1917 Specifications
 No. of Spans 2

PLATE DIMEN	Remarks	E 70 LL+I+DL	E 70 LL+I	100/1	N.Y.C. Allowable LL+I	Rating 100/1	Rating 50/1
Web							
Unit S.							
Bear.							
Compr.							
Bear.							
Riv.							
Web Splice Riv.							
	(End 5)						
	(Act. 3)						
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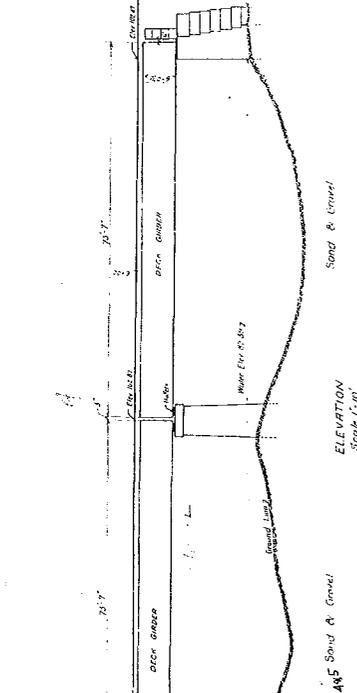
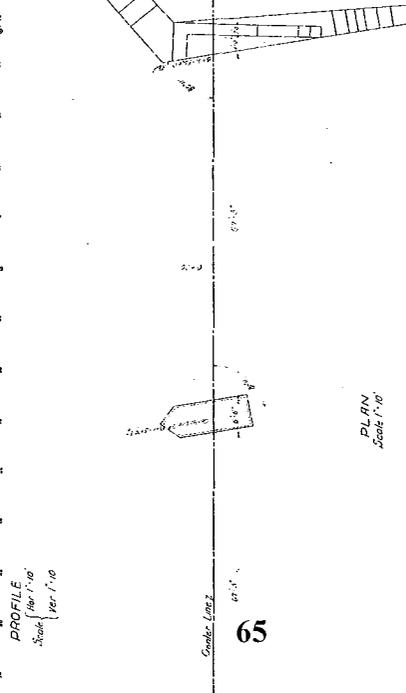
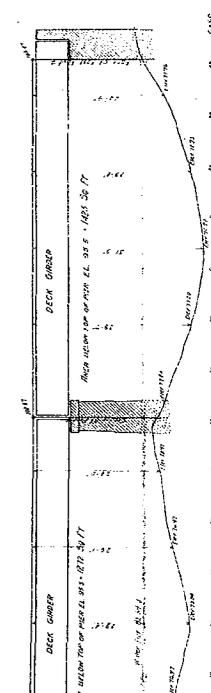
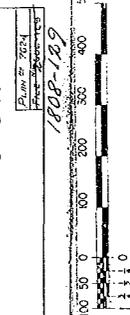


Approved: *[Signature]*
 Board of Park Commissioners

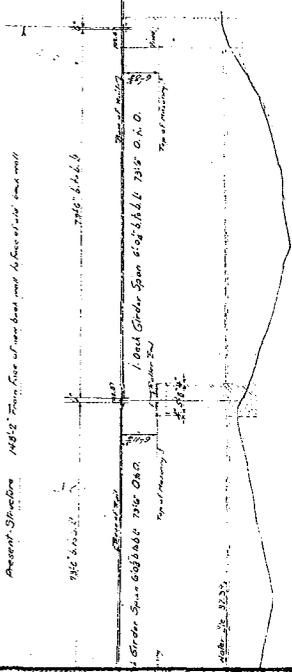
MP 241243



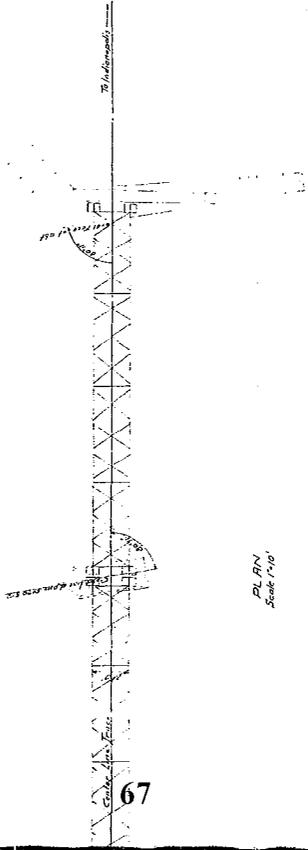
C.C.C. & ST. L. RY.
 PROPOSED EXTENSION OF MILL CREEK BRIDGE
 BRIDGE 498 CHICAGO DIVISION
 INDIANAPOLIS, IND.
 Chief Engineer
 MF 241243
 BR 2.79



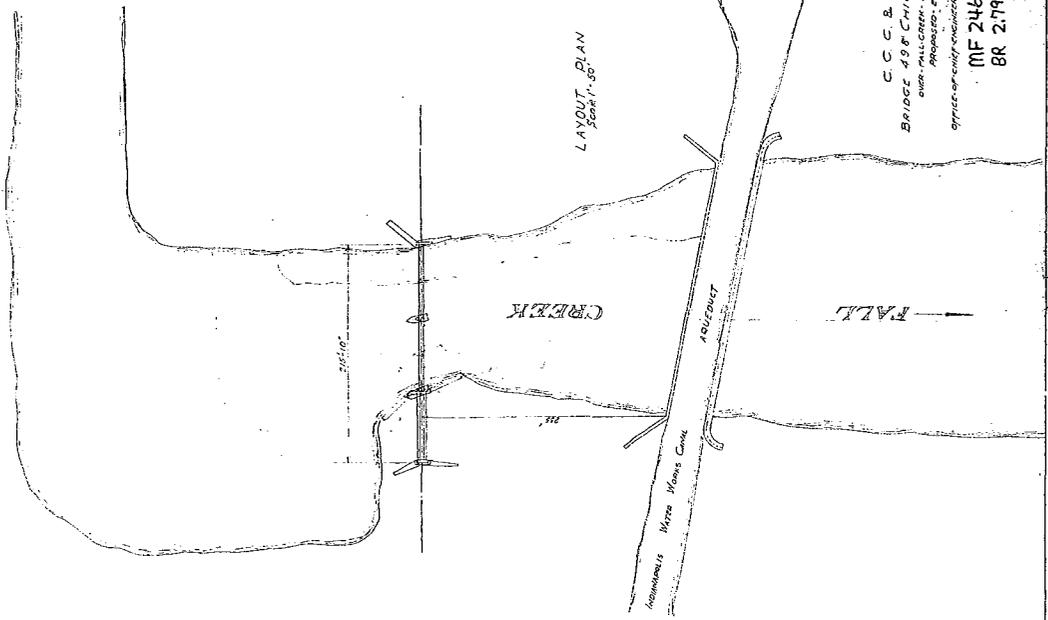
Approved: *[Signature]*
 Chief Engineer
 B. M. F. 241243
 BR 2.79



Section



PLAN
 Scale 1"=10'

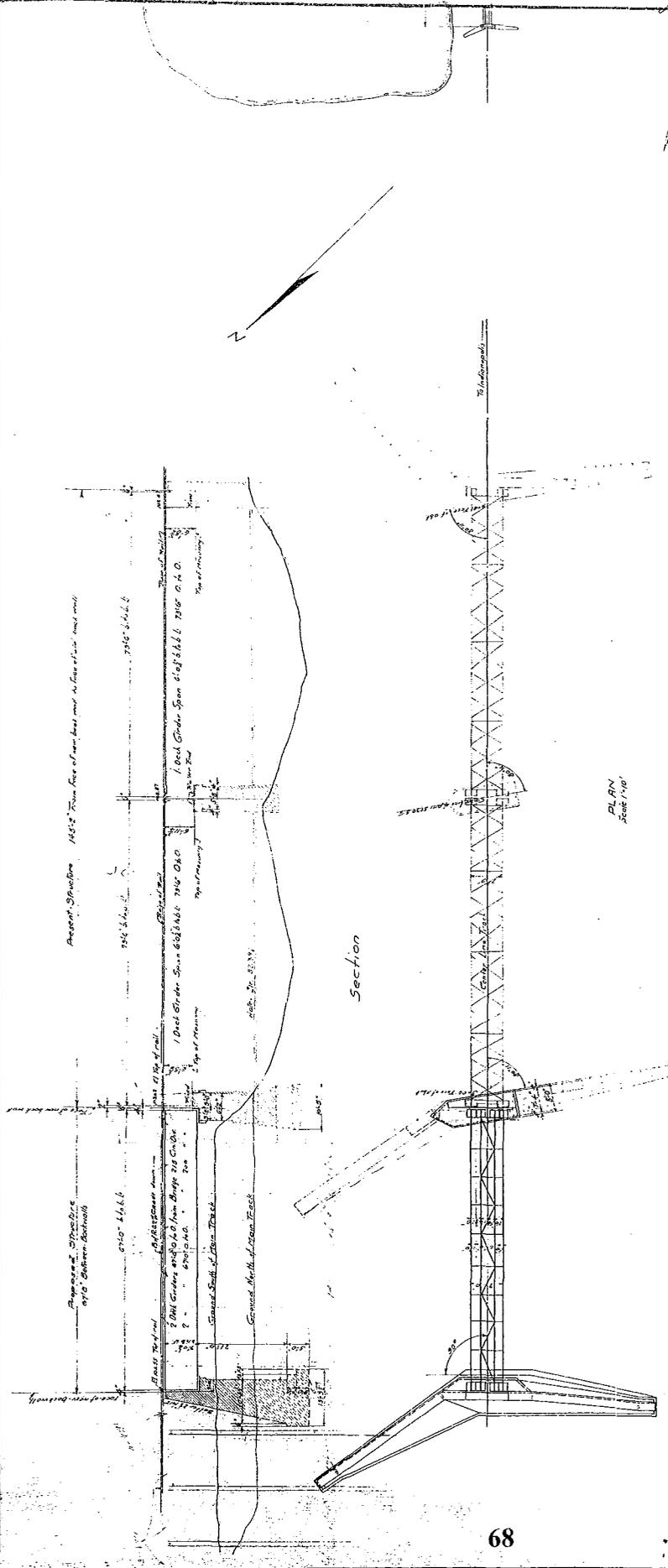


C. C. & ST. LRY.
 BRIDGE AND CHICAGO DIVISION
 OVER FALL CREEK, ILLINOIS
 PROJECT NO. 2179
 MF 246328
 BR 2179

FILE NO. 106-20



Fd



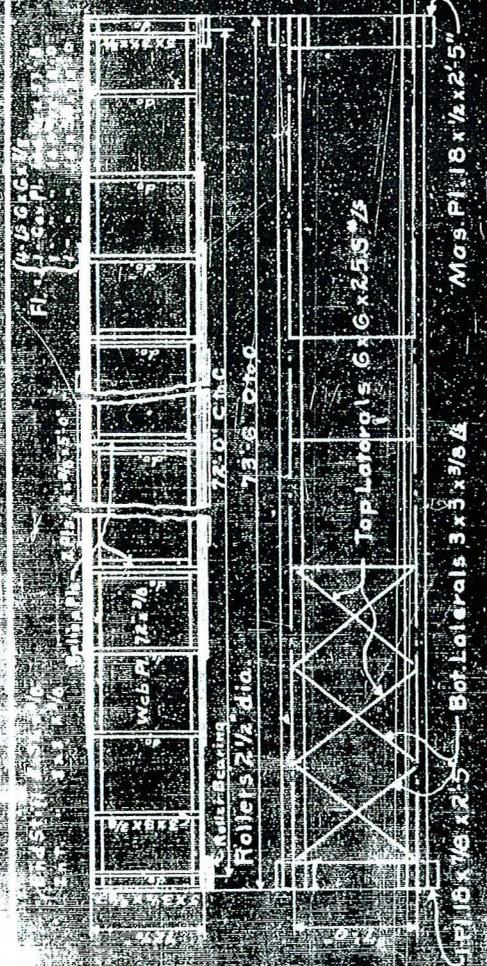
Proposed Structure
10' x 10' Grid

Section

PLAN
Scale 1/10'

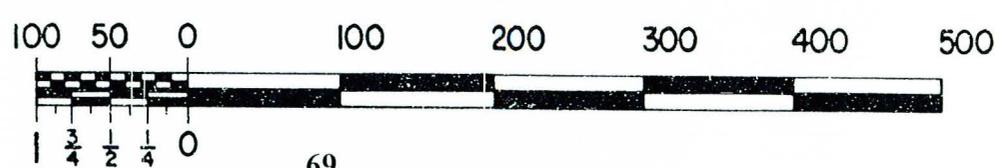
MF 246328

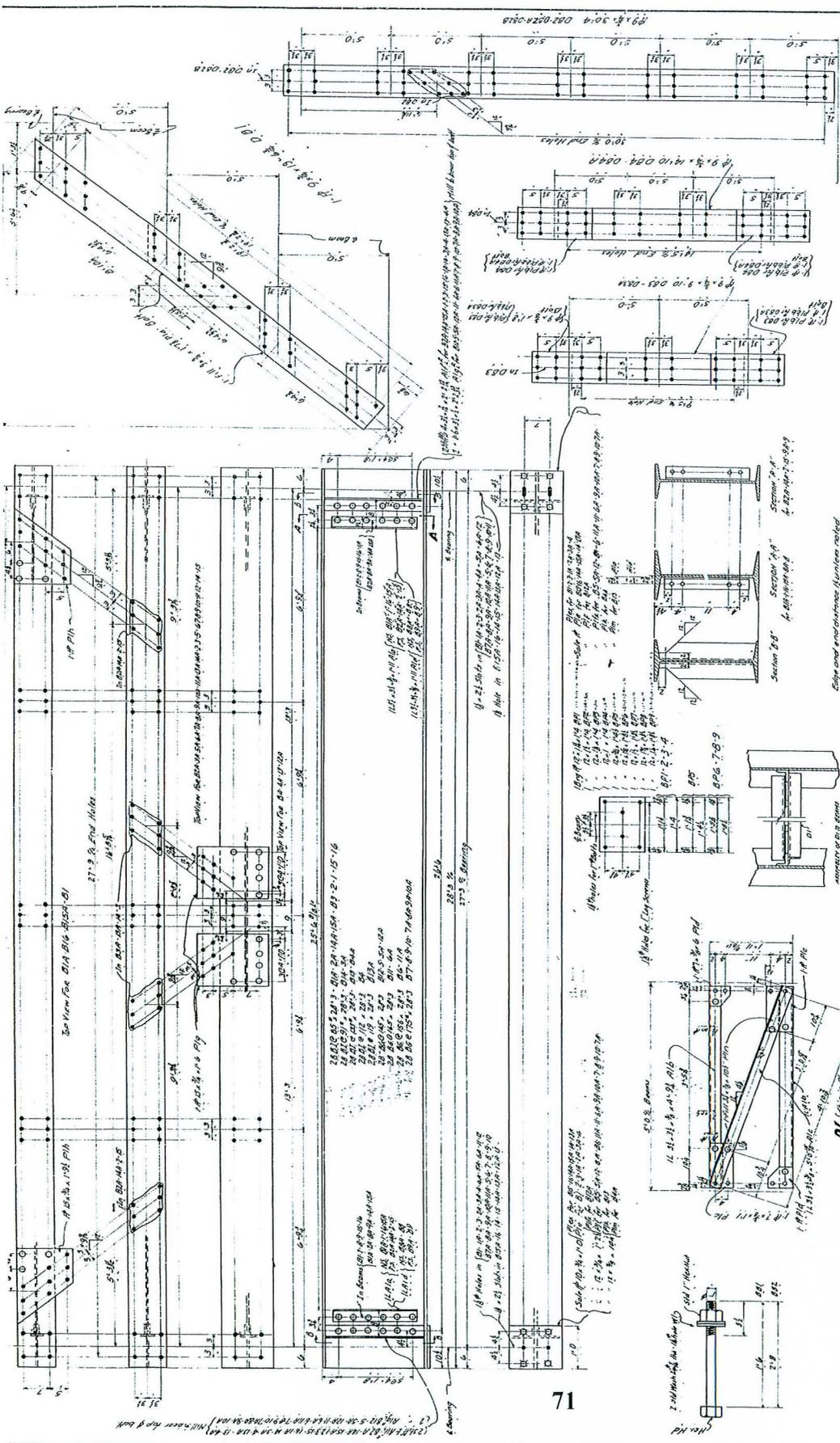
Order No. 199
 Division Chicago Div
 Date from Plans 1915-19
 Mfr. Massillon Br. Co.
 Date 1895
 Material Soft Steel
 Rivets 7/8
 End Rivets 3 x 3/2 x 3/8
 Inter Rivets 3 x 3 x 3/8
 Web 7/2 x 3/8
 Flanges 6 x 6 x 1 1/8 x 5
 I-Beam 14 1/2 x 35.5 - 1-Cov Pl. 14 x
 7/8 x 48.1 - Cov. Pl. 14 x 96 x 57.5
 I of Section Av. F. Method
 Impacts 1917 Specifications
 No. of Spans 2
 Rated - 1919



Remarks	E 70 LL + I	DL	E 70 LL + I	Allowable LL + I	Rating	New AISC Rating	NYC Rating	Max. Net Area	ARE.A. E-70 LL + I	Allow. LL + I
Unit 1	395600	28200	367400	16600	64.3	13950	71.5	13600	13955	71.6
Net Area 208	19200	1400	17800	31900	42.5	34000	45.0	52800	40024000	45.4
Dist. 696	56800	4000	52800	18700	43.0	14660	33.7	39400	2300	4700
Comp. 11-6	32700	2300	30400	33500	71.3	35500	75.5	32900	2500	35500
Dist. 26-3 Rivets	52200	3700	48500	32300	46.6	34300	49.5	48500	3700	34300
(End 5)	13600	1800	11800	36000	185.0	36110	86.3	13600	1890	16110
Net Area 30.7	Greater than Web									
Dist. 265-00	2000	2000	26600	34000	89.5	36000	95.0	26500	2000	36000
Net Area 52.2	75.9	54	70.5	44.6	44.3	44.6	44.3			
Dist. 65000	4500	4500	60500	22000	25.4					
Dist. 2890	210	210	2780	2040	51.4					
Dist. 132	132	132	132							
Dist. 507000	507000	507000	507000							
Dist. 27400	27400	27400	27400	19800	55.0	19800	55.0	25700	27400	19800
Dist. 23900	18900	18900	21700	11200	56.8	18900	59.4	21200	18900	14770
Dist. 1/4 = 54.3										
Dist. O.K.										

MF 257124
 BL 2.79





THE MT. VERNON BRIDGE CO.
 IN CHARGE OF THE MT. VERNON BRIDGE CO.
 MADE BY THE MT. VERNON BRIDGE CO.
 CHECKED BY: [Signature]
 SHEET NO. 5575
 CONT. NO. BR-3.10

REQUIRED	QUANTITY	REQUIRED	QUANTITY	REQUIRED	QUANTITY	REQUIRED	QUANTITY
Beam Section	1						
Component of Beam	1						
2- Diaphragms	2						
1- Beam	1						
2- Diaphragms	2						
1- Beam	1						
2- Diaphragms	2						
1- Beam	1						
2- Diaphragms	2						

Edge and end-connections (unless noted)
 Rivets and end-connections (unless noted)

100 50 0 100 200 300 400 500

THE
MT. VERNON BRIDGE COMPANY

CONTRACT No. 5578

SHEET No. A2

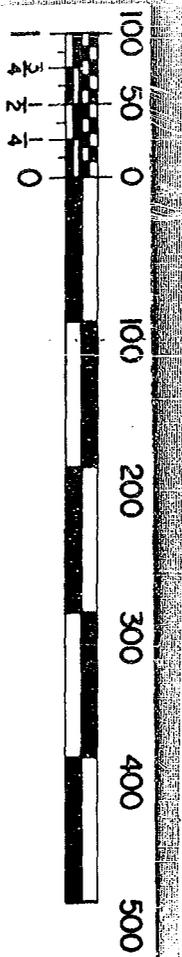
2-21 1930

Erectors Summary LIST OF FIELD RIVETS

MADE BY J.W.V.

CHK by J.S.S.

Number	Size	Grip	Length	LOCATION
16	$\frac{7}{8}$	$2\frac{3}{4}$	4	B7, B10 to DB2A, DB4A thru P16b
36	"	$2\frac{3}{4}$	4	" " " DB2, 2B, DB4 " " "
16	"	$2\frac{3}{4}$	4	B7A, B9A to DB2A, DB3A thru P16b
36	"	$2\frac{3}{4}$	4	" " " DB2, 2B, DB3
6A	"	$1\frac{1}{2}$	$3\frac{1}{8}$	DB1 to DB2
52	"	$1\frac{1}{2}$	$3\frac{5}{8}$	B52 to DBA, DB4A
26	"	$1\frac{1}{2}$	$3\frac{5}{8}$	B8A to DB3, DB3A



Summary

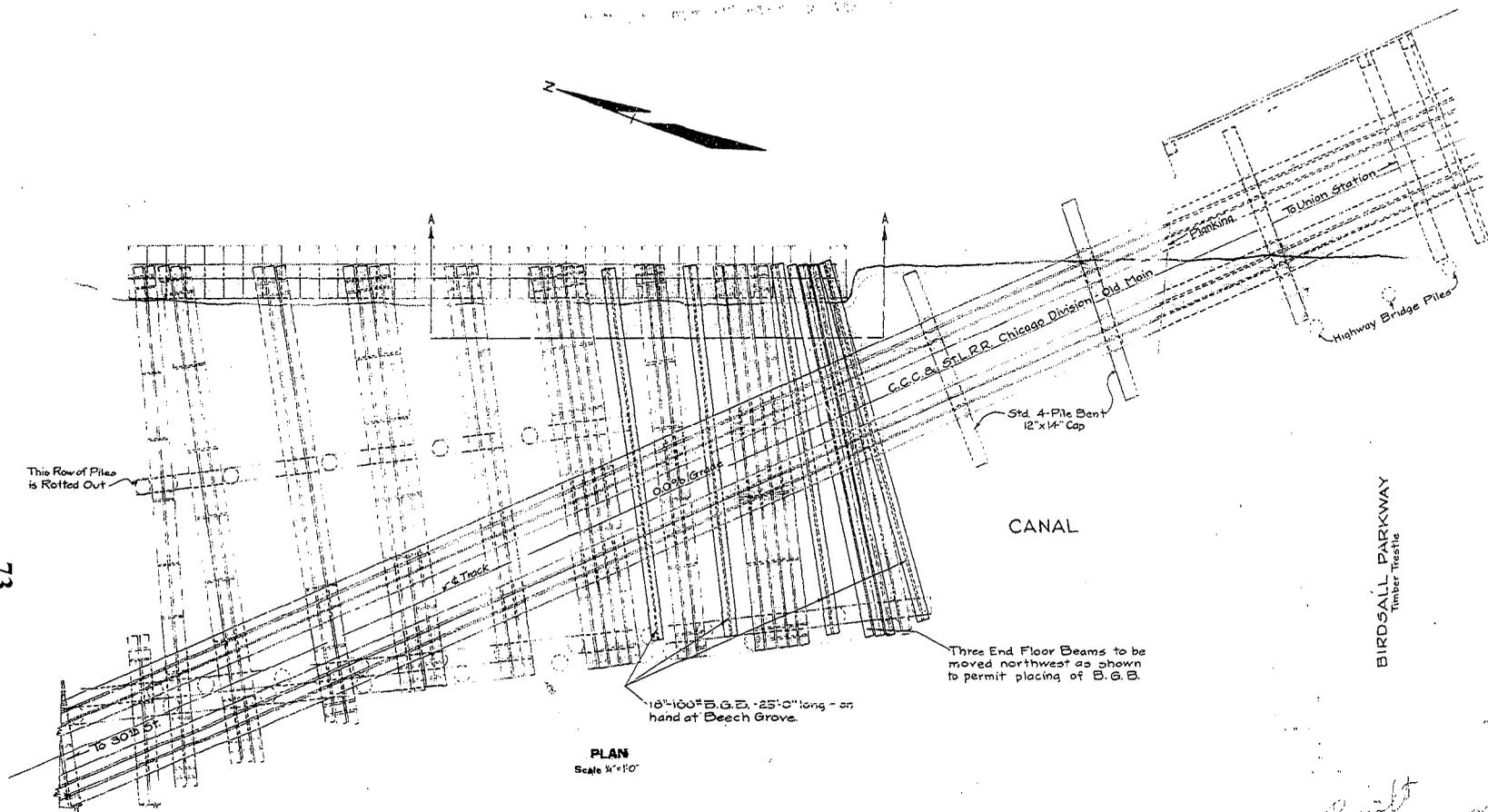
✓ 10	$\frac{7}{8}$	$4\frac{3}{8}$	Button Head Rivets.
✓ 20	"	$4\frac{1}{4}$	
✓ 10	"	$4\frac{1}{8}$	
✓ 150	"	4	
✓ 125	"	$3\frac{7}{8}$	
✓ 180	"	$3\frac{5}{8}$	
✓ 150	"	$3\frac{1}{2}$	
✓ 150	"	$3\frac{1}{4}$	
✓ 215	"	$3\frac{1}{8}$	
✓ 270	"	$2\frac{3}{8}$	
✓ 120	"	$2\frac{5}{8}$	

MF 243454

BR 3.10

Plan 27883
File 1718-30

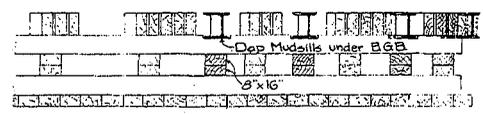
73



Three End Floor Beams to be moved northwest as shown to permit placing of B. B. B.

18"-100# B. B. B. - 25'-0" long - on hand at Beech Grove.

PLAN
Scale 1/4" = 1'-0"



SECTION "A-A"
Scale 3/4" = 1'-0"

*as Built
for N.B.H. Co.
Little - 6-20-1924
D. W. Johnson
MF 246240
BR 3.10*

THE CLEVELAND, CINCINNATI, CHICAGO & ST. LOUIS RY. CO.

BRIDGE NO. 499 CHICAGO DIVISION OLD MAIN STA. 157 + 00

REINFORCING PILE TRESTLE WITH
BETHELEM GIRDER BEAMS

Office of Chief Engineer
Cincinnati, Ohio: Jan. 19, 1924

Sheet 1 of 1

DRAWN BY: *K. G. ...*
CHECKED BY: *J. ...*
FILE NO.: 21898

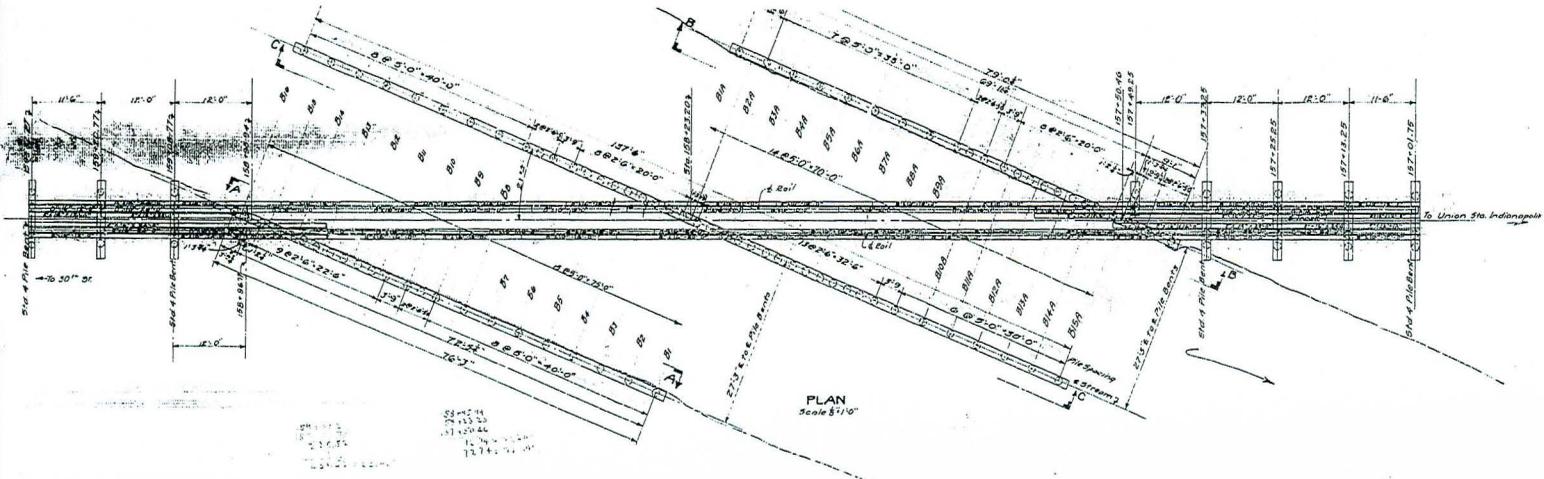
CORRECTED BY: _____ ENGINEER

APPROVED BY: _____ CHIEF ENGINEER

REVISIONS		
DATE	BY	DESCRIPTION

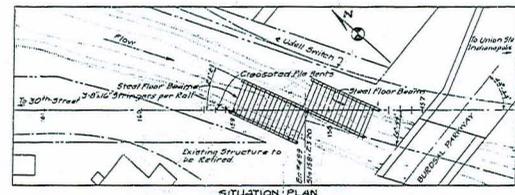
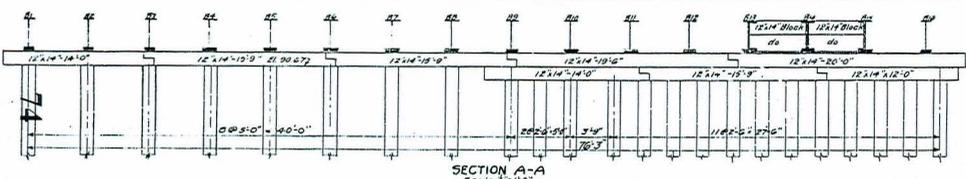
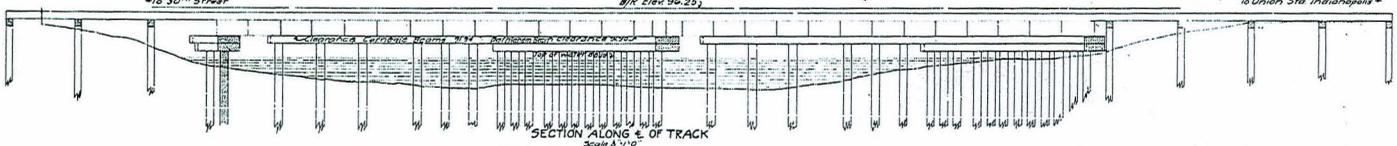


917-31



BILL OF MATERIAL

SECTION	LENGTH IN	NO. PIECES	S.B.P.	LOCATION IN STRUCTURE
12"x14"	10	1	140	Caps Under Steel Beams
12"x14"	15	2	330	"
12"x14"	14	3	358	"
12"x14"	16	14	1136	"
12"x14"	18	1	252	"
12"x14"	20	6	1080	"
12"x14"	15	6	1244	Caps for Std. 2 1/2" Dia. Pile Bents
12"x14"	8	1	112	Caps for 3" Dia. Bents
12"x14"	23	2	640	Between Beams 12"x14" & 12"x10"
8"x16"	12	3	640	Stringers
8"x16"	74	2	300	"
8"x16"	16	29	4920	"
8"x16"	18	3	576	"
8"x16"	20	4	692	"
8"x16"	22	1	232	"
8"x16"	24	16	4096	"
8"x16"	20	1	208	"
2"x14"	10	5	187	Cut in 2' 0" lengths Cap Sills
2"x14"	12	1	28	"
			20520	



BOLT SCHEDULE

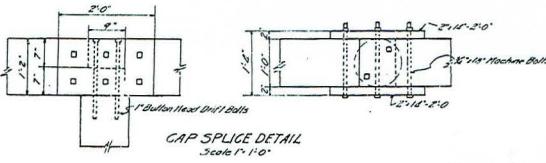
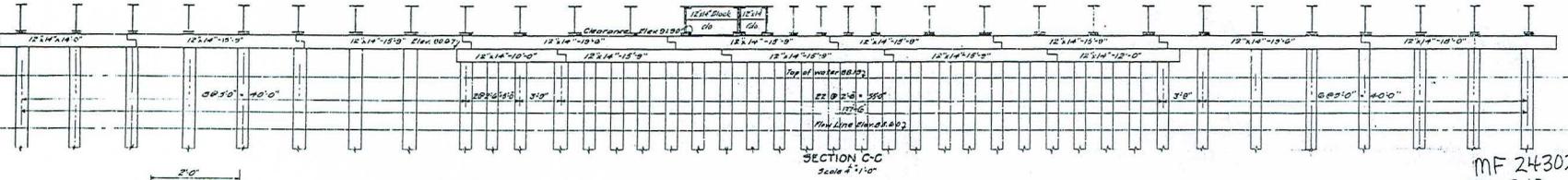
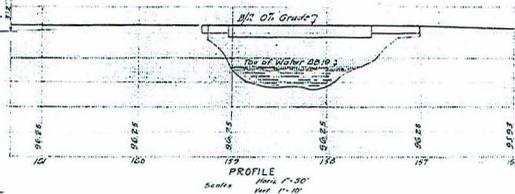
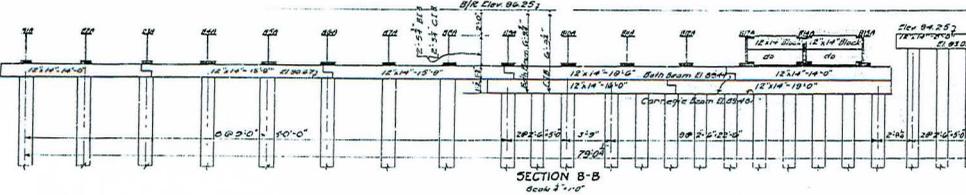
TYPE	MARK NO.	NO. OF BOLTS	LOCATION
Machine	A	150 3/4"	1. 6 Flat 2. 6 For each Cap Splice
Button Head	M	75 1 1/2"	1 Single Caps to Piles
		70 1 1/2"	1 Double
		70 3/4"	1 Stringers to Caps
Chord		60 3/4"	2 Connecting 3rd Stringer
		100 3/4"	1 10 5/16" 1 2 Ply

APPROXIMATE QUANTITIES

MATERIAL	AMOUNT
Timber	21000 S.F.
Structural Steel (Carnegie)	21500 Lbs.
" (Bethlehem)	10200
Piles	120

LIST OF OTHER PLANS

DESCRIPTION	PLAN NO.	FILE NO.
Stress Sheet	21425	1112-28



MF 243020
BR 3.10

THE CLEVELAND, CINCINNATI, CHICAGO & ST. LOUIS RY. CO.
DIV. SEC. 1
BRIDGE NO. 499 CHICAGO DIVISION - OLD MAIN - STA. 158+23.20
STEEL & TIMBER TRESTLE
OVER BROAD CUT CANAL
FRAMING PLAN

Scale as shown

Office of Civil Engineer
Cleveland, Ohio, Dec. 16, 1929

Sheet 1 of 1

DATE: 12/16/29
BY: R.S. 10/10/29
CHECKED: R.S. 10/10/29
APPROVED: R.S. 10/10/29

REVISIONS

NO.	DATE	DESCRIPTION
1	12/16/29	As shown

PLAN NO. 21425
FILE NO. 1006-11



THE
MT. VERNON BRIDGE COMPANY

CONTRACT No. 5578

SHEET No. A1

2-21, 1930

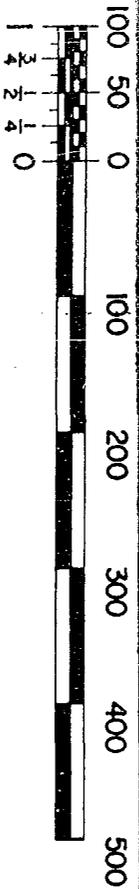
Erectors

LIST OF FIELD RIVETS

MADE BY J.W. C. by J.S.S.

Number Size Grip Length LOCATION

48	$\frac{7}{8}$	$2\frac{1}{2}$	$3\frac{7}{8}$	DB1 to B51, B53, B54, B55 thru P1h
24	"	$1\frac{1}{2}$	$3\frac{1}{8}$	DB1 to P1h
48	"	$2\frac{1}{2}$	$3\frac{7}{8}$	DB1 to B51, B53, B54, B55
24	"	$2\frac{1}{4}$	4	DB1 to B1A, B3A
12	"	$2\frac{1}{2}$	$3\frac{7}{8}$	" " B3
12	"	$2\frac{1}{2}$	$4\frac{1}{4}$	" " B13A
40	"	$1\frac{1}{2}$	$3\frac{1}{8}$	" " P1g
2	"	$2\frac{1}{2}$	4h	" " " thru B4
4	"	$2\frac{3}{8}$	$4\frac{3}{8}$	" " " " B4A, B13
2	"	$2\frac{3}{8}$	4h	" " " " B12A
234	"	$1\frac{3}{8}$	$2\frac{7}{8}$	DB2A, DB2, DB2B to B51, 3, 4, 5 B3
52	"	$1\frac{1}{2}$	$3\frac{1}{8}$	" " " " B14, B3A
52	"	$1\frac{7}{8}$	$3\frac{5}{8}$	" " " " B13, B4A
78	"	$1\frac{3}{4}$	$3\frac{1}{2}$	" " " " B13A, B11, 6A
104	"	$1\frac{5}{8}$	$3\frac{1}{4}$	" " " " B12, 5, 5A, 12A
52	"	$1\frac{1}{2}$	$3\frac{1}{2}$	" " " " B6, 11A
26	"	$1\frac{1}{2}$	$3\frac{5}{8}$	" " " " B10A
16	"	$1\frac{1}{2}$	$2\frac{5}{8}$	DB2A to P1bb
36	"	$1\frac{1}{2}$	$2\frac{5}{8}$	DB2, DB2B to P1b
8	"	$1\frac{1}{2}$	$2\frac{5}{8}$	DB4A to P1bb
18	"	$1\frac{1}{2}$	$2\frac{5}{8}$	DB4 to P1b
8	"	$1\frac{1}{2}$	$2\frac{5}{8}$	DB3A to P1bb
18	"	$1\frac{1}{2}$	$2\frac{5}{8}$	DB3 to P1b
26	"	$1\frac{1}{2}$	$3\frac{1}{4}$	DB2A, DB2, DB2B to B4



Handwritten notes and calculations on the right side of the page, including various numbers and symbols like '13', '14', '15', '16', '17', '18', '19', '20', '21', '22', '23', '24', '25', '26', '27', '28', '29', '30', '31', '32', '33', '34', '35', '36', '37', '38', '39', '40', '41', '42', '43', '44', '45', '46', '47', '48', '49', '50', '51', '52', '53', '54', '55', '56', '57', '58', '59', '60', '61', '62', '63', '64', '65', '66', '67', '68', '69', '70', '71', '72', '73', '74', '75', '76', '77', '78', '79', '80', '81', '82', '83', '84', '85', '86', '87', '88', '89', '90', '91', '92', '93', '94', '95', '96', '97', '98', '99', '100'. There are also some symbols like 'x', 'y', 'z', 'a', 'b', 'c', 'd', 'e', 'f', 'g', 'h', 'i', 'j', 'k', 'l', 'm', 'n', 'o', 'p', 'q', 'r', 's', 't', 'u', 'v', 'w', 'x', 'y', 'z'.

MF 243455

BR 3.10

Plan 27884
File 1718-31.

BETHLEHEM BEAM SCHEDULE

MARK AND LENGTH	KIND	LOADS		SECTION	STRESSES		REMARKS	
		SHEAR	MOMENT		KIND	ALLOWABLE		ACTUAL
B1, B2, B3	DL	3.10	14.54	Required S.M. 1880 m ³	Tension	18000	12700	10" x 3/4" Sole R
B15, B16	LL	31.20	147.20		Comp	15000	12700	4" x 3 1/2" x 7/16" STIFF PL
B14, B2A	I	15.60	73.60	27 1/8" - 85" B.I.B.	Shear	13500	4080	8 Shop Rivets
B14A, B15A	Total	49.90	235.34	S.M. 2221 m ³				16" x 1 1/4" Bearing R
	DL	5.68	16.60	Required S.M. 236 m ³	Tension	11000	14430	10" x 3/4" Sole R
*B14	LL	77.60	187.0		Comp	15120	14430	4" x 3 1/2" x 7/16" STIFF PL
B3A	I	38.80	93.5	27 1/8" - 91" B.I.B.	Shear	13500	9740	8 Shop Rivets
	Total	122.08	297.10	S.M. 246.9 m ³				16" x 1 1/4" Bearing R
	DL	5.70	211	Required S.M. 298.0 m ³	Tension	18000	14800	10" x 3/4" Sole R
B4	LL	73.43	238.0		Comp	15240	14800	4" x 3 1/2" x 7/16" STIFF PL
	I	36.72	119.0	28 1/4" - 112" B.I.B.	Shear	13500	7680	8 Shop Rivets
	Total	115.85	378.1	S.M. 306.4 m ³				16" x 1 1/4" Bearing R
	DL	6.52	22.90	Required S.M. 315 m ³	Tension	18000	14710	10" x 3/4" Sole R
B13A	LL	86.20	251.80		Comp	15260	14710	4" x 3 1/2" x 7/16" STIFF PL
	I	43.10	125.90	28 1/8" - 119" B.I.B.	Shear	13500	8475	6 Shop Rivets
	Total	135.82	400.60	S.M. 327.5 m ³				16" x 1" Bearing R
	DL	5.38	24.50	Required S.M. 340 m ³	Tension	18000	14250	10 1/2" x 3/4" Sole R
B3	LL	61.20	272.50		Comp	15300	14250	4" x 3 1/2" x 7/16" STIFF PL
*B4A	I	30.60	136.20	28 1/2" - 133" B.I.B.	Shear	13500	5400	6 Shop Rivets
	Total	97.18	433.20	S.M. 364 m ³				16 1/2" x 3/4" Bearing R
	DL	5.30	32.6	Required S.M. 406 m ³	Tension	18000	15700	14 1/2" x 3/4" Sole R
B5, B12	LL	56.00	342.0		Comp	16110	15700	6" x 3 1/2" x 1/2" STIFF PL
B5A, B12A	I	28.00	171.0	27 3/4" - 145" B.G.B.	Shear	13500	5500	6 Shop Rivets
	Total	84.30	345.6	S.M. 416.0 m ³				20 1/2" x 1 3/8" Bearing R
	DL	5.34	37.4	Required S.M. 439.0 m ³	Tension	18000	15900	14 1/2" x 3/4" Sole R
B6	LL	53.10	368.0		Comp	16120	15900	6" x 3 1/2" x 1/2" STIFF PL
*B11A	I	26.55	184.0	27 1/8" - 156" B.G.B.	Shear	13500	4810	6 Shop Rivets
	Total	84.99	589.4	S.M. 446.1 m ³				20 1/2" x 1 1/2" Bearing R
	DL	5.94	39.0	Required S.M. 463 m ³	Tension	18000	15800	14 1/2" x 3/4" Sole R
*B11	LL	50.40	389.0		Comp	16140	15800	6" x 3 1/2" x 1/2" STIFF PL
B6A	I	25.20	194.5	28" - 165" B.G.B.	Shear	13500	4300	6 Shop Rivets
	Total	80.99	622.5	S.M. 473.2 m ³				20 1/2" x 1 3/8" Bearing R
B7, B8, B9	DL	4.93	40.7	Required S.M. 493 m ³	Tension	18000	15900	14 1/2" x 3/4" Sole R
B10	LL	42.10	415.0		Comp	16150	15900	6" x 3 1/2" x 1/2" STIFF PL
B7A, B9A	I	21.05	207.5	28 1/8" - 175" B.G.B.	Shear	13500	3420	6 Shop Rivets
*B9A, B10A	Total	68.08	663.2	S.M. 494.7 m ³				20 1/2" x 1 1/4" Bearing R

Estimated weight of Span = 150,000 lbs
 Note - Unit Stresses, Material, Workmanship and Painting as per N.Y.C. Line Specifications for Steel Railroad Bridges 1917. Moments are in 1000 ft. lbs. - Shears are in 1000 lbs. Rivets 7/8"; Holes 1 1/4" unless otherwise noted.

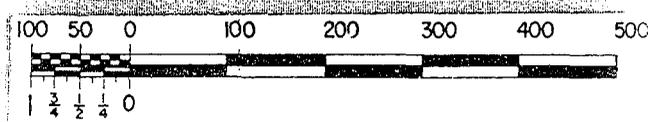
Loads and Stresses in Table are for beams marked *

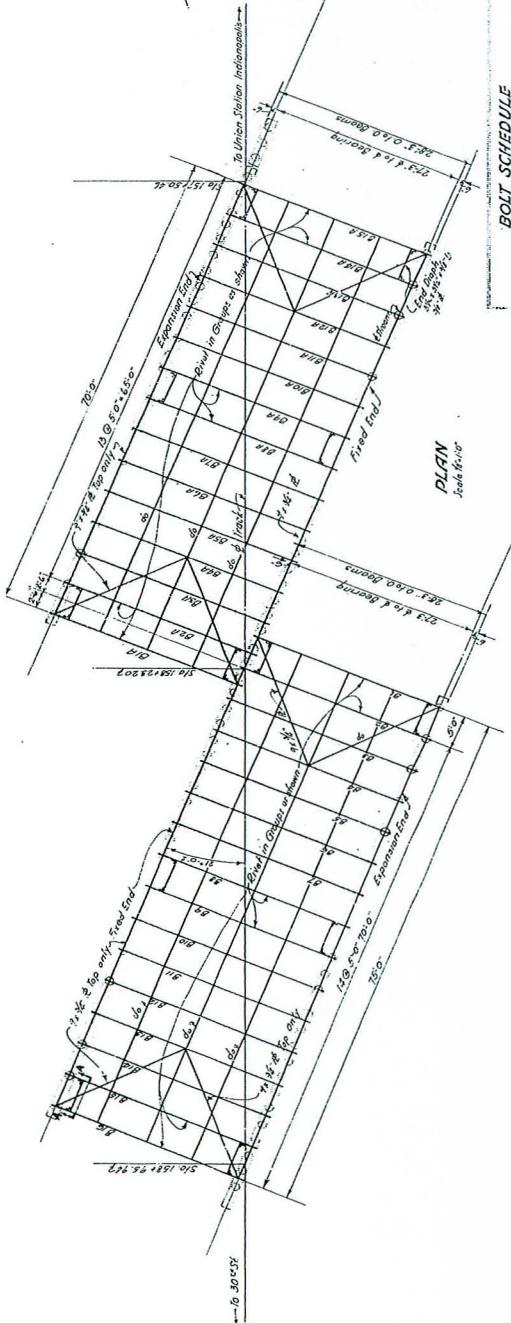
CORRECT
 ENGINEER BRIDGES & STRUCTURES
 APPROVED
 CHIEF ENGINEER

REVISIONS		
DATE	BY	DESCRIPTION

THE CLEVELAND, CINCINNATI, CHICAGO & ST. LOUIS RY. CO.
 VAL. SEC. 1 SHEET NO. 23
BRIDGE NO. 499 CHICAGO DIVISION - OLD MAIN - STA 158 - 23.20
STEEL & TIMBER TRESTLE
OVER BROAD CUT CANAL MF 243456
STRESS SHEET BR 3.10

Scales as shown
 Sheet 2 of 2
 Office of Chief Engineer
 Cincinnati, Ohio, Dec. 16, 1929
 DRAWN BY *W. J. ...* PLAN NO. 27466
 TRACED BY *W. J. ...* FILE NO. 1712-48R
 CHECKED BY *W. J. ...*

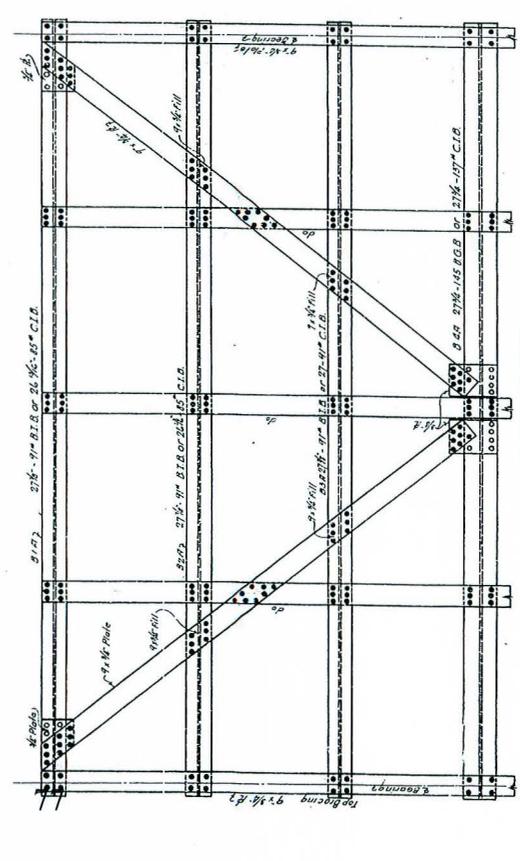




BOLT SCHEDULE
 TYPE: A325
 SIZE: 1/2"
 SPACING: 6" ON CENTER
 END SPACING: 3" ON CENTER
 EDGE SPACING: 1 1/2" ON CENTER
 END DISTANCE: 1 1/2" ON CENTER
 END DISTANCE: 1 1/2" ON CENTER

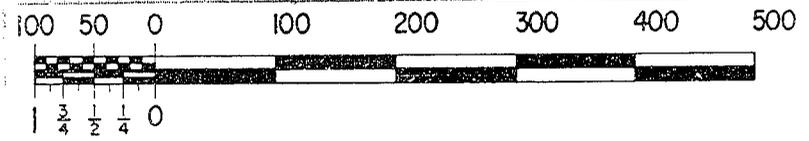
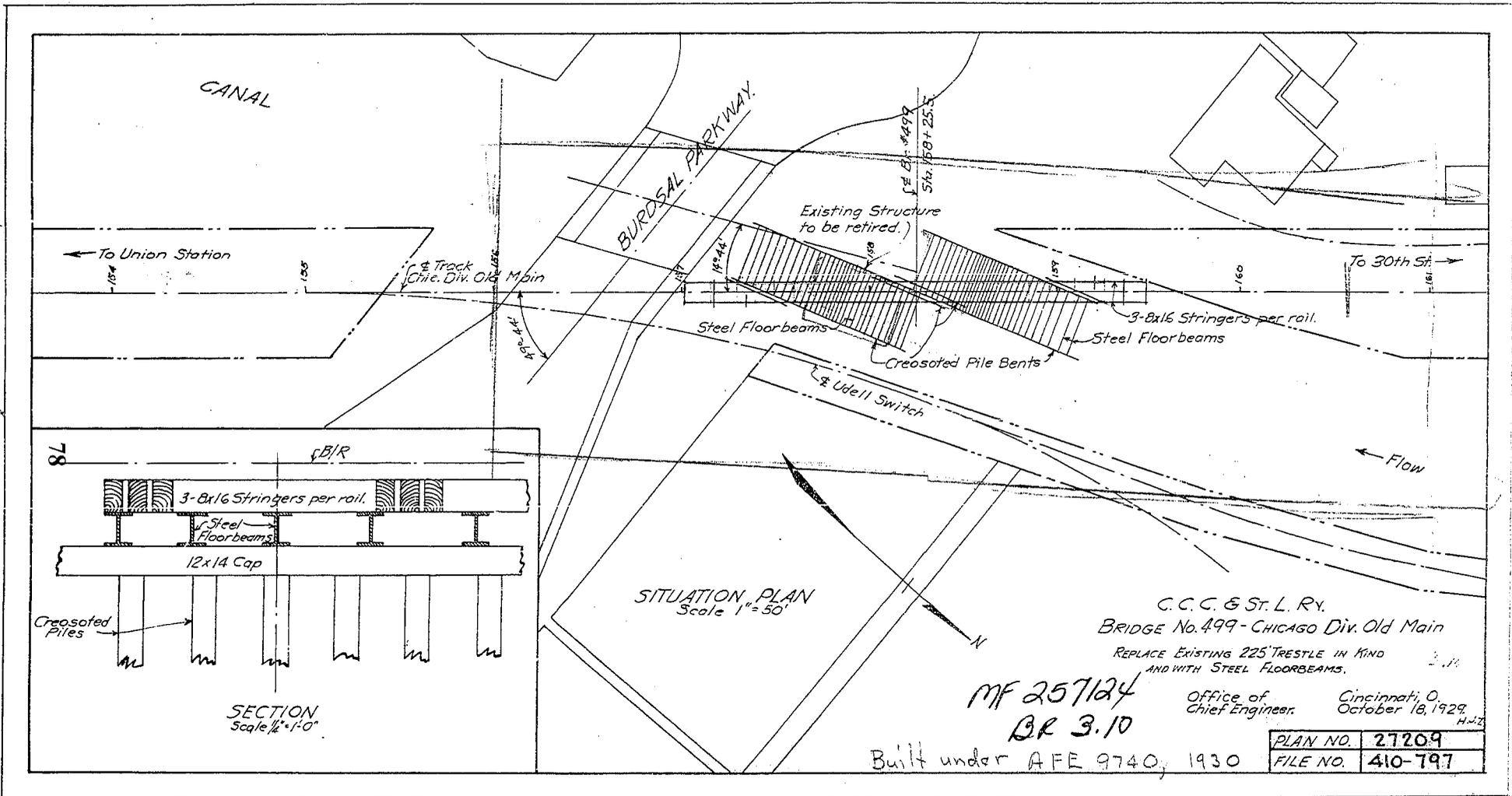
BEAM SCHEDULE (CARNegie)
 MEMBER: 24" x 40" I-beam
 SPACING: 20' ON CENTER
 TOTAL LENGTH: 100' ON CENTER
 END DISTANCE: 5' ON CENTER

DEAD LOAD 600 lbs. Rv. Lim. 10 ft.
LIVE LOAD ETD 50%
Impact 50%



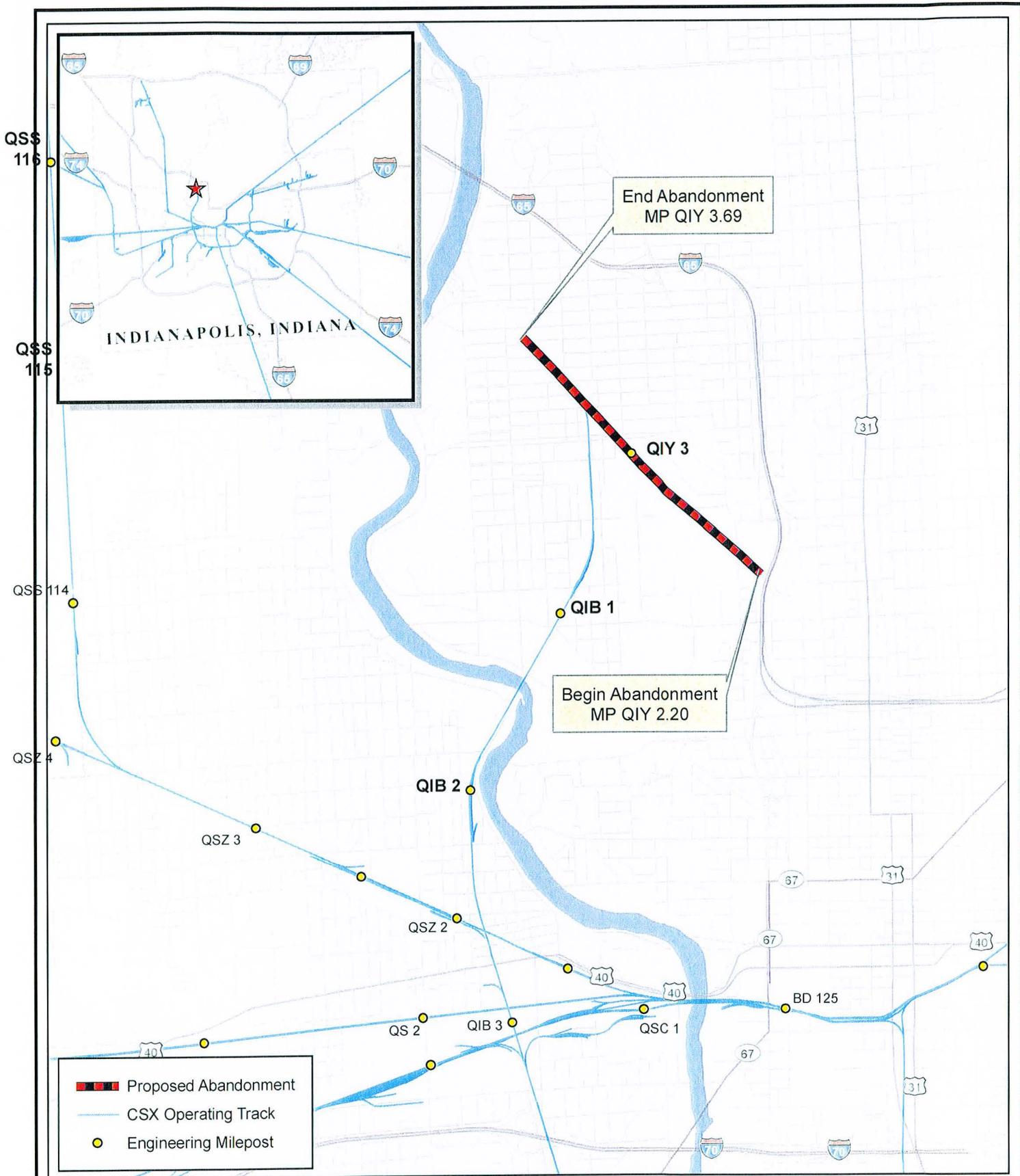
BRACING DETAIL
 JOB NO. 12-1-10

Member	Length	Area	Weight	Section	Stresses	Remarks
24" x 40" I	100.00	14.40	144.00	24" x 40" I	10000	1st 3/4' Sale d.
24" x 40" I	114.00	16.32	163.20	24" x 40" I	11400	4 1/2' Sale d.
24" x 40" I	128.00	18.24	182.40	24" x 40" I	12800	6 1/2' Sale d.
24" x 40" I	142.00	20.16	201.60	24" x 40" I	14200	8 1/2' Sale d.
24" x 40" I	156.00	22.08	220.80	24" x 40" I	15600	10 1/2' Sale d.
24" x 40" I	170.00	24.00	240.00	24" x 40" I	17000	12 1/2' Sale d.
24" x 40" I	184.00	25.92	259.20	24" x 40" I	18400	14 1/2' Sale d.
24" x 40" I	198.00	27.84	278.40	24" x 40" I	19800	16 1/2' Sale d.
24" x 40" I	212.00	29.76	297.60	24" x 40" I	21200	18 1/2' Sale d.
24" x 40" I	226.00	31.68	316.80	24" x 40" I	22600	20 1/2' Sale d.
24" x 40" I	240.00	33.60	336.00	24" x 40" I	24000	22 1/2' Sale d.
24" x 40" I	254.00	35.52	355.20	24" x 40" I	25400	24 1/2' Sale d.
24" x 40" I	268.00	37.44	374.40	24" x 40" I	26800	26 1/2' Sale d.
24" x 40" I	282.00	39.36	393.60	24" x 40" I	28200	28 1/2' Sale d.
24" x 40" I	296.00	41.28	412.80	24" x 40" I	29600	30 1/2' Sale d.
24" x 40" I	310.00	43.20	432.00	24" x 40" I	31000	32 1/2' Sale d.
24" x 40" I	324.00	45.12	451.20	24" x 40" I	32400	34 1/2' Sale d.
24" x 40" I	338.00	47.04	470.40	24" x 40" I	33800	36 1/2' Sale d.
24" x 40" I	352.00	48.96	489.60	24" x 40" I	35200	38 1/2' Sale d.
24" x 40" I	366.00	50.88	508.80	24" x 40" I	36600	40 1/2' Sale d.
24" x 40" I	380.00	52.80	528.00	24" x 40" I	38000	42 1/2' Sale d.
24" x 40" I	394.00	54.72	547.20	24" x 40" I	39400	44 1/2' Sale d.
24" x 40" I	408.00	56.64	566.40	24" x 40" I	40800	46 1/2' Sale d.
24" x 40" I	422.00	58.56	585.60	24" x 40" I	42200	48 1/2' Sale d.
24" x 40" I	436.00	60.48	604.80	24" x 40" I	43600	50 1/2' Sale d.
24" x 40" I	450.00	62.40	624.00	24" x 40" I	45000	52 1/2' Sale d.
24" x 40" I	464.00	64.32	643.20	24" x 40" I	46400	54 1/2' Sale d.
24" x 40" I	478.00	66.24	662.40	24" x 40" I	47800	56 1/2' Sale d.
24" x 40" I	492.00	68.16	681.60	24" x 40" I	49200	58 1/2' Sale d.
24" x 40" I	506.00	70.08	700.80	24" x 40" I	50600	60 1/2' Sale d.
24" x 40" I	520.00	72.00	720.00	24" x 40" I	52000	62 1/2' Sale d.
24" x 40" I	534.00	73.92	739.20	24" x 40" I	53400	64 1/2' Sale d.
24" x 40" I	548.00	75.84	758.40	24" x 40" I	54800	66 1/2' Sale d.
24" x 40" I	562.00	77.76	777.60	24" x 40" I	56200	68 1/2' Sale d.
24" x 40" I	576.00	79.68	796.80	24" x 40" I	57600	70 1/2' Sale d.
24" x 40" I	590.00	81.60	816.00	24" x 40" I	59000	72 1/2' Sale d.
24" x 40" I	604.00	83.52	835.20	24" x 40" I	60400	74 1/2' Sale d.
24" x 40" I	618.00	85.44	854.40	24" x 40" I	61800	76 1/2' Sale d.
24" x 40" I	632.00	87.36	873.60	24" x 40" I	63200	78 1/2' Sale d.
24" x 40" I	646.00	89.28	892.80	24" x 40" I	64600	80 1/2' Sale d.
24" x 40" I	660.00	91.20	912.00	24" x 40" I	66000	82 1/2' Sale d.
24" x 40" I	674.00	93.12	931.20	24" x 40" I	67400	84 1/2' Sale d.
24" x 40" I	688.00	95.04	950.40	24" x 40" I	68800	86 1/2' Sale d.
24" x 40" I	702.00	96.96	969.60	24" x 40" I	70200	88 1/2' Sale d.
24" x 40" I	716.00	98.88	988.80	24" x 40" I	71600	90 1/2' Sale d.
24" x 40" I	730.00	100.80	1008.00	24" x 40" I	73000	92 1/2' Sale d.
24" x 40" I	744.00	102.72	1027.20	24" x 40" I	74400	94 1/2' Sale d.
24" x 40" I	758.00	104.64	1046.40	24" x 40" I	75800	96 1/2' Sale d.
24" x 40" I	772.00	106.56	1065.60	24" x 40" I	77200	98 1/2' Sale d.
24" x 40" I	786.00	108.48	1084.80	24" x 40" I	78600	100 1/2' Sale d.
24" x 40" I	800.00	110.40	1104.00	24" x 40" I	80000	102 1/2' Sale d.
24" x 40" I	814.00	112.32	1123.20	24" x 40" I	81400	104 1/2' Sale d.
24" x 40" I	828.00	114.24	1142.40	24" x 40" I	82800	106 1/2' Sale d.
24" x 40" I	842.00	116.16	1161.60	24" x 40" I	84200	108 1/2' Sale d.
24" x 40" I	856.00	118.08	1180.80	24" x 40" I	85600	110 1/2' Sale d.
24" x 40" I	870.00	120.00	1200.00	24" x 40" I	87000	112 1/2' Sale d.
24" x 40" I	884.00	121.92	1219.20	24" x 40" I	88400	114 1/2' Sale d.
24" x 40" I	898.00	123.84	1238.40	24" x 40" I	89800	116 1/2' Sale d.
24" x 40" I	912.00	125.76	1257.60	24" x 40" I	91200	118 1/2' Sale d.
24" x 40" I	926.00	127.68	1276.80	24" x 40" I	92600	120 1/2' Sale d.
24" x 40" I	940.00	129.60	1296.00	24" x 40" I	94000	122 1/2' Sale d.
24" x 40" I	954.00	131.52	1315.20	24" x 40" I	95400	124 1/2' Sale d.
24" x 40" I	968.00	133.44	1334.40	24" x 40" I	96800	126 1/2' Sale d.
24" x 40" I	982.00	135.36	1353.60	24" x 40" I	98200	128 1/2' Sale d.
24" x 40" I	996.00	137.28	1372.80	24" x 40" I	99600	130 1/2' Sale d.
24" x 40" I	1010.00	139.20	1392.00	24" x 40" I	101000	132 1/2' Sale d.
24" x 40" I	1024.00	141.12	1411.20	24" x 40" I	102400	134 1/2' Sale d.
24" x 40" I	1038.00	143.04	1430.40	24" x 40" I	103800	136 1/2' Sale d.
24" x 40" I	1052.00	144.96	1449.60	24" x 40" I	105200	138 1/2' Sale d.
24" x 40" I	1066.00	146.88	1468.80	24" x 40" I	106600	140 1/2' Sale d.
24" x 40" I	1080.00	148.80	1488.00	24" x 40" I	108000	142 1/2' Sale d.
24" x 40" I	1094.00	150.72	1507.20	24" x 40" I	109400	144 1/2' Sale d.
24" x 40" I	1108.00	152.64	1526.40	24" x 40" I	110800	146 1/2' Sale d.
24" x 40" I	1122.00	154.56	1545.60	24" x 40" I	112200	148 1/2' Sale d.
24" x 40" I	1136.00	156.48	1564.80	24" x 40" I	113600	150 1/2' Sale d.
24" x 40" I	1150.00	158.40	1584.00	24" x 40" I	115000	152 1/2' Sale d.
24" x 40" I	1164.00	160.32	1603.20	24" x 40" I	116400	154 1/2' Sale d.
24" x 40" I	1178.00	162.24	1622.40	24" x 40" I	117800	156 1/2' Sale d.
24" x 40" I	1192.00	164.16	1641.60	24" x 40" I	119200	158 1/2' Sale d.
24" x 40" I	1206.00	166.08	1660.80	24" x 40" I	120600	160 1/2' Sale d.
24" x 40" I	1220.00	168.00	1680.00	24" x 40" I	122000	162 1/2' Sale d.
24" x 40" I	1234.00	169.92	1699.20	24" x 40" I	123400	164 1/2' Sale d.
24" x 40" I	1248.00	171.84	1718.40	24" x 40" I	124800	166 1/2' Sale d.
24" x 40" I	1262.00	173.76	1737.60	24" x 40" I	126200	168 1/2' Sale d.
24" x 40" I	1276.00	175.68	1756.80	24" x 40" I	127600	170 1/2' Sale d.
24" x 40" I	1290.00	177.60	1776.00	24" x 40" I	129000	172 1/2' Sale d.
24" x 40" I	1304.00	179.52	1795.20	24" x 40" I	130400	174 1/2' Sale d.
24" x 40" I	1318.00	181.44	1814.40	24" x 40" I	131800	176 1/2' Sale d.
24" x 40" I	1332.00	183.36	1833.60	24" x 40" I	133200	178 1/2' Sale d.
24" x 40" I	1346.00	185.28	1852.80	24" x 40" I	134600	180 1/2' Sale d.
24" x 40" I	1360.00	187.20	1872.00	24" x 40" I	136000	182 1/2' Sale d.
24" x 40" I	1374.00	189.12	1891.20	24" x 40" I	137400	184 1/2' Sale d.
24" x 40" I	1388.00	191.04	1910.40	24" x 40" I	138800	186 1/2' Sale d.
24" x 40" I	1402.00	192.96	1929.60	24" x 40" I	140200	188 1/2' Sale d.
24" x 40" I	1416.00	194.88	1948.80	24" x 40" I	141600	190 1/2' Sale d.
24" x 40" I	1430.00	196.80	1968.00	24" x 40" I	143000	192 1/2' Sale d.
24" x 40" I	1444.00	198.72	1987.20	24" x 40" I	144400	194 1/2' Sale d.
24" x 40" I	1458.00	200.64	2006.40	24" x 40" I	145800	196 1/2' Sale d.
24" x 40" I	1472.00	202.56	2025.60	24" x 40" I	147200	198 1/2' Sale d.
24" x 40" I	1486.00	204.48	2044.80	24" x 40" I	148600	200 1/2' Sale d.
24" x 40" I	1500.00	206.40	2064.00	24" x 40" I	150000	202 1/2' Sale d.
24" x 40" I	1514.00	208.32	2083.20	24" x 40" I	151400	204 1/2' Sale d.
24" x 40" I	1528.00	210.24	2102.40	24" x 40" I	152800	206 1/2' Sale d.
24" x 40" I	1542.00	212.16	2121.60	24" x 40" I	154200	208 1/2' Sale d.
24" x 40" I	1556.00	214.08	2140.80	24" x 40" I	155600	210 1/2' Sale d.
24" x 40" I	1570.00	216.00	2160.00	24" x 40" I	157000	212 1/2' Sale d.
24" x 40" I	1584.00	217.92	2179.20	24" x 40" I	158400	214 1/2' Sale d.
24" x 40" I	1598.00	219.84	2198.40	24" x 40" I	159800	216 1/2' Sale d.
24" x 40" I	1612.00	221.76	2217.60	24" x 40" I	161200	218 1/2' Sale d.
24" x 40" I	1626.00	223.68	2236.80	24" x 40" I	162600	220 1/2' Sale d.
24" x 40" I	1640.00	225.60	2256.00	24" x 40" I	164000	222 1/2' Sale d.
24" x 40" I	1654.00	227.52	2275.20	24" x 40" I	165400	224 1/2' Sale d.
24" x 40" I	1668.00	229.44	2294.40	24" x 40" I	166800	226 1/2' Sale d.
24" x 40" I	1682.00	231.36	2313.60	24" x 40" I	168200	228 1/2' Sale d.
24" x 40" I	1696.00	233.28	2332.80	24" x 40" I	169600	230 1/2' Sale d.
24" x 40" I	1710.00	235.20	2352.00	24" x 40" I	171000	232 1/2' Sale d.
24" x 40" I	1724.00	237.12				



2x

EXHIBIT F—COLOR EXHIBITS



-  Proposed Abandonment
-  CSX Operating Track
-  Engineering Milepost



CSX Real Property, Inc.

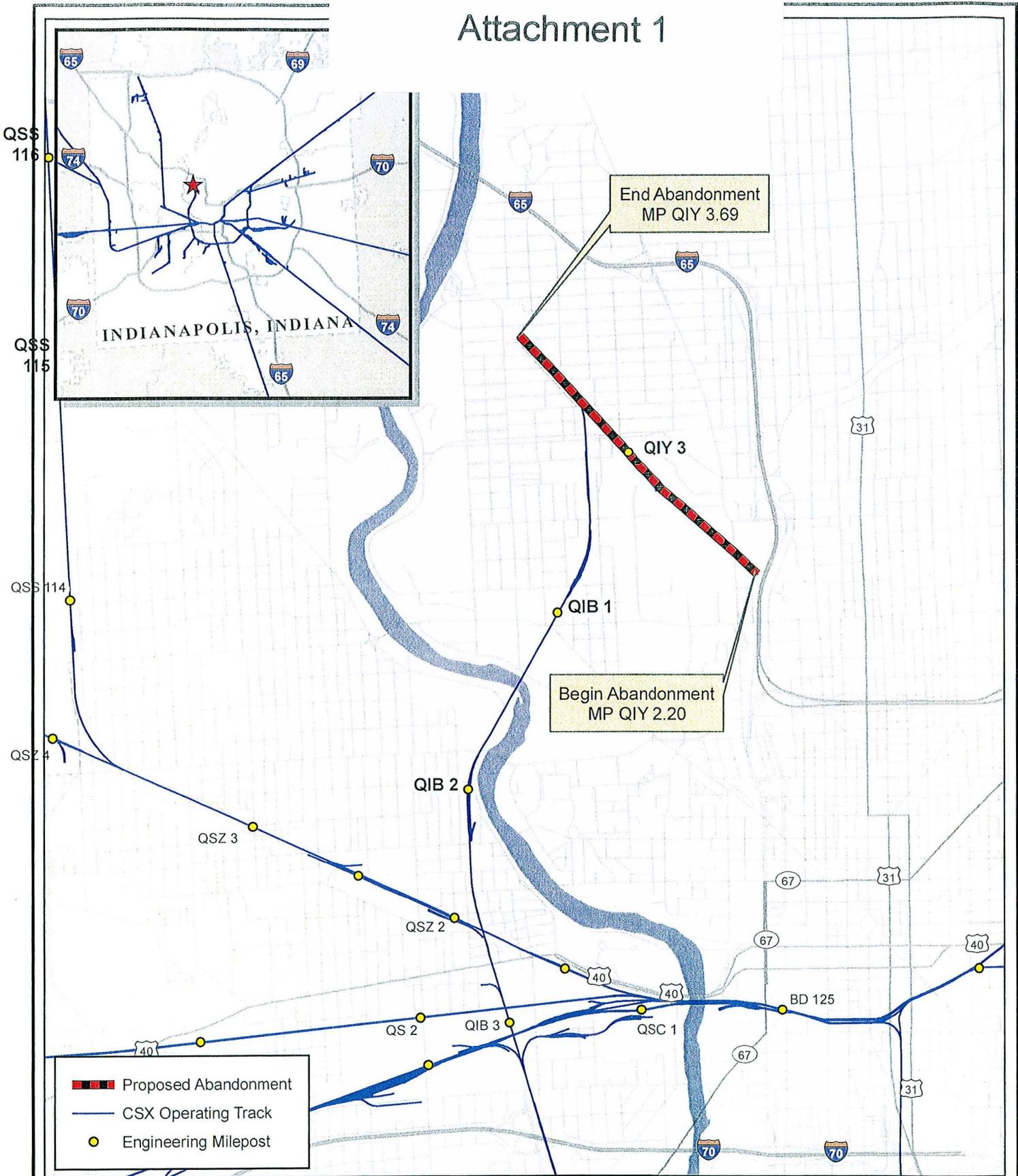
CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB 55 (Sub-No. 709X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana



Feet

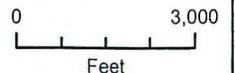


Attachment 1



CSX Real Property, Inc.

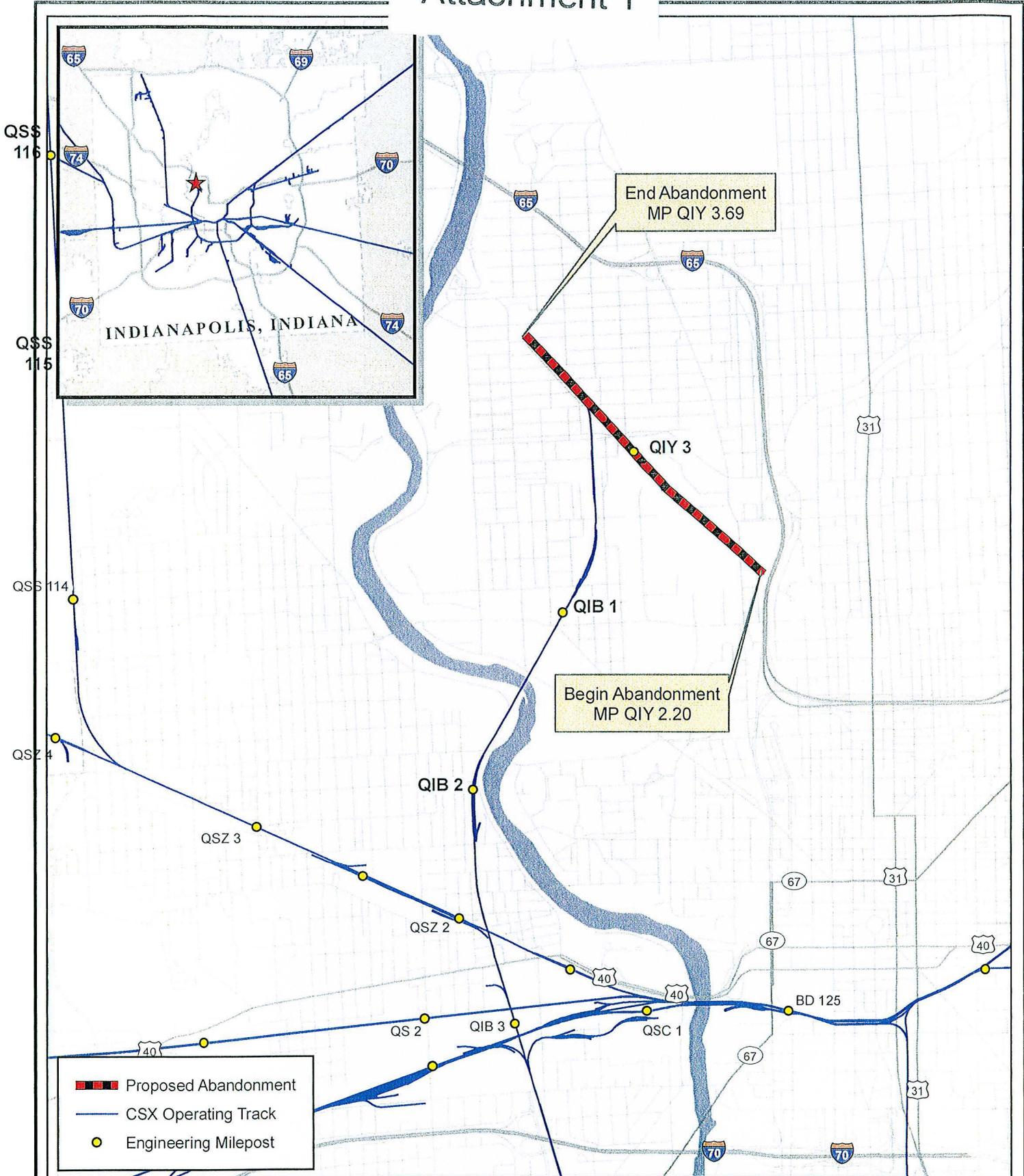
CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB 55 (Sub-No. 709X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana



Feet



Attachment 1



CSX Real Property, Inc.

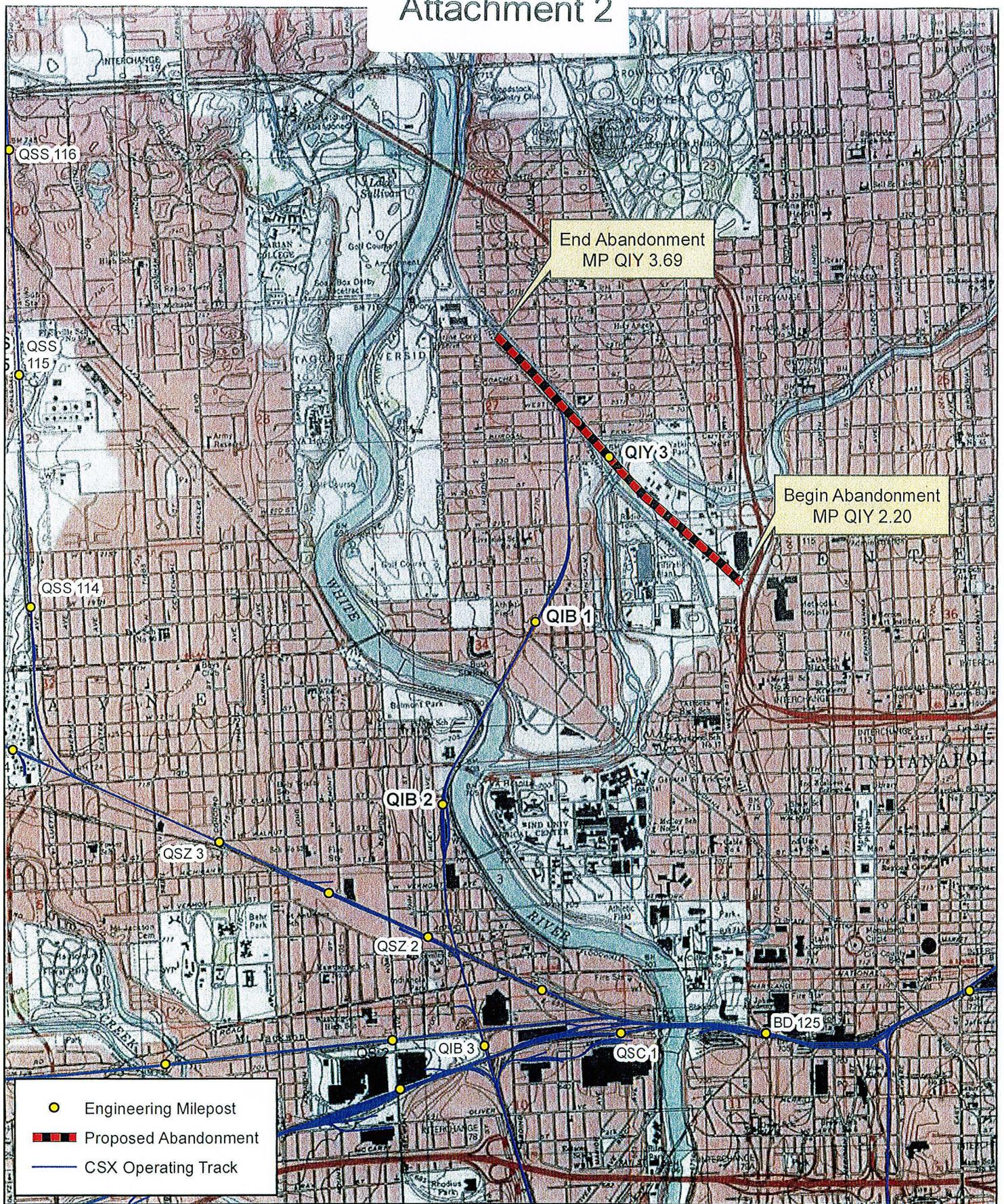
CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB 55 (Sub-No. 709X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana

0 3,000

Feet



Attachment 2



- Engineering Milepost
- ▬ Proposed Abandonment
- ▬ CSX Operating Track



CSX Real Property, Inc.

CSX Transportation, Inc.
Proposed Abandonment
QIY 3.69 to QIY 2.20 (1.49 Miles Total)
STB Docket No. AB55 (Sub No. 709 X)
Great Lakes Division - Indianapolis Terminal Subdivision
Indianapolis, Marion County, Indiana

0 3,000

Feet



Attachment 3
Bridge 498
Milepost QIY 2.79



Attachment 3
Bridge 498
Milepost QIY 2.79



Attachment 4
Bridge 499
Milepost QIY 3.10



Attachment 4
Bridge 499
Milepost QIY 3.10

