



David T. Rankin
Senior General Attorney

BNSF Railway Company
PO Box 961039
Fort Worth, TX 76161
2500 Lou Menk Drive – AOB-3
Fort Worth, TX 76131-2828
817-352-2383
817-352-2398 fax

David.Rankin@BNSF.com

VIA UPS OVERNIGHT MAIL

May 11, 2011

229503

Ms. Victoria Rutson
Chief, Office of Environmental Analysis
Surface Transportation Board
395 E Street S.W.
Washington, DC 20423-0001



**Re: STB Docket No. AB-6 (Sub-No. 477X); BNSF Railway Company –
Abandonment Exemption – in Los Angeles County, California**

Dear Ms. Rutson:

Enclosed for filing in STB Docket No. AB-6 (Sub-No. 477X) are the original and ten copies of BNSF Railway Company's Environmental and Historic Reports.

BNSF anticipates filing a Petition for Exemption seeking authority to abandon the 4.85-mile rail line on or after May 31, 2011.

Sincerely,


David T. Rankin
Senior General Attorney

ENTERED
Office of Proceedings

MAY 12 2011

Part of
Public Record

Enclosures: As stated

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May 11, 2011

U.S. Department of the Interior
Bureau of Land Management
California Field Office
2800 Cottage Way, Suite W-1623
Sacramento, CA 95825-1886



Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment Exemption – in Los Angeles County, California

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

We are providing these reports so that you may review the information that will form the basis for the STB's independent environmental and historic analyses of this proceeding. If any of the information is misleading or incorrect, if you believe that pertinent information is missing, or if you have any questions about the STB's environmental review process, please contact the Office of Environmental Analysis (OEA), Surface Transportation Board, 395 E Street S.W., Washington, D.C. 20423-0001, telephone (202) 245-0305, and refer to the above docket number. Because the applicable statutes and regulations impose stringent deadlines for processing this action, your written comments to OEA, with a copy to BNSF, would be appreciated within three weeks.

Your comments will be considered by the STB in evaluating the environmental and/or historic preservation impacts of the contemplated action. If there are any questions concerning these reports, please contact John Sims at 817-352-2376 or john.sims@bnsf.com or me at 817-352-2383 or david.rankin@bnsf.com.

Sincerely,

David T. Rankin
Senior General Attorney

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Enclosures as stated



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David.Rankin@BNSF.com

May 11, 2011

City of Arcadia
Planning Commission
240 West Huntington Dr.
P.O. Box 60021
Arcadia, CA 91066

Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment Exemption – in Los Angeles County, California

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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Sincerely,

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817-352-2383
817-352-2398 fax

David.Rankin@BNSF.com

May 11, 2011

City of Irwindale
Planning Commission
5050 North Irwindale Avenue
Irwindale, CA 91706

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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Sincerely,

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817-352-2398 fax

David.Rankin@BNSF.com

May 11, 2011

NOAA
National Geodetic Survey
VIA E-mail: NGS.InfoCenter@noaa.gov

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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Fort Worth, TX 76131-2828
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817-352-2398 fax

David.Rankin@BNSF.com

May 11, 2011

California Department of Water Resources
1416 9th Street
Sacramento, CA 95814

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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Fort Worth, TX 76131-2828
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David.Rankin@BNSF.com

May 11, 2011

U.S. EPA Region 9
75 Hawthorne Street
San Francisco, CA 94105

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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David.Rankin@BNSF.com

May 11, 2011

U.S. Fish & Wildlife Service
Sacramento Fish & Wildlife Office
2800 Cottage Way, Room W-2605
Sacramento, CA 95825

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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Fort Worth, TX 76131-2828
817-352-2383
817-352-2398 fax

David.Rankin@BNSF.com

May 11, 2011

Mr. Richard J. Bruckner, Director
Department of Regional Planning
County of Los Angeles
1390 Hall of Records
320 West Temple Street
Los Angeles, CA 90012

Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment Exemption – in Los Angeles County, California

Dear Mr. Bruckner,

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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David T. Rankin
Senior General Attorney

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Enclosures as stated



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David.Rankin@BNSF.com

May 11, 2011

Ms. Christine Lehnertz, Regional Director
U.S. Department of the Interior
National Park Service
One Jackson Center
1111 Jackson Street, Suite 700
Oakland, CA 94607

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

Dear Ms. Lehnertz,

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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David.Rankin@BNSF.com

May 11, 2011

Mr. Lincoln E. Burton, State Conservationist
California NRCS State Office
430 G Street #4164
Davis, CA 95616-4164

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

Dear Mr. Burton,

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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David.Rankin@BNSF.com

May 11, 2011

U.S. Army Corps of Engineers
Los Angeles District
915 Wilshire Blvd., Suite 1101
Los Angeles, CA 90017

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

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David.Rankin@BNSF.com

May 11, 2011

Mr. Milford Wayne Donaldson, FAIA, State Historic Preservation Officer
California State Parks
Office of Historic Preservation
1725 23rd Street, Suite 100
Sacramento, CA 95816

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

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May 11, 2011

California Department
of Transportation
P.O. Box 942873
Sacramento, CA 94273-0001

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

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Your comments will be considered by the STB in evaluating the environmental and/or historic preservation impacts of the contemplated action. If there are any questions concerning these reports, please contact John Sims at 817-352-2376 or john.sims@bnsf.com or me at 817-352-2383 or david.rankin@bnsf.com.

Sincerely,

David T. Rankin
Senior General Attorney

DTR/js

Enclosures as stated



David T. Rankin
Senior General Attorney

BNSF Railway Company
PO Box 961039
Fort Worth, TX 76161
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Fort Worth, TX 76131-2828
817-352-2383
817-352-2398 fax

David.Rankin@BNSF.com

May 11, 2011

California Public
Utilities Commission
505 Van Ness Avenue
San Francisco, CA 94102

**Re: STB Docket No. AB-6 (Sub-No. 477X) - BNSF Railway Company – Abandonment
Exemption – in Los Angeles County, California**

In approximately three weeks BNSF will be filing with the Surface Transportation Board (STB) a Petition for Exemption seeking authority to abandon BNSF's Rail Freight Service Easement over 4.85 miles of rail line located in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale. The line traverses United States Postal ZIP Codes 91006, 91007, 91010, 91016, 91702, and 91706. Enclosed are environmental and historic reports describing the proposed action and any expected environmental or historic effects, as well as a map of the affected area.

We are providing these reports so that you may review the information that will form the basis for the STB's independent environmental and historic analyses of this proceeding. If any of the information is misleading or incorrect, if you believe that pertinent information is missing, or if you have any questions about the STB's environmental review process, please contact the Office of Environmental Analysis (OEA), Surface Transportation Board, 395 E Street S.W., Washington, D.C. 20423-0001, telephone (202) 245-0305, and refer to the above docket number. Because the applicable statutes and regulations impose stringent deadlines for processing this action, your written comments to OEA, with a copy to BNSF, would be appreciated within three weeks.

Your comments will be considered by the STB in evaluating the environmental and/or historic preservation impacts of the contemplated action. If there are any questions concerning these reports, please contact John Sims at 817-352-2376 or john.sims@bnsf.com or me at 817-352-2383 or david.rankin@bnsf.com.

Sincerely,

David T. Rankin
Senior General Attorney

DTR/js

Enclosures as stated

**BEFORE THE
SURFACE TRANSPORTATION BOARD**

BNSF RAILWAY COMPANY)
ABANDONMENT EXEMPTION)
IN LOS ANGELES COUNTY,)
CALIFORNIA

DOCKET NO. AB-6
(SUB-NO. 477X)

ENVIRONMENTAL REPORT

BNSF RAILWAY COMPANY
2650 Lou Menk Drive
P.O. Box 96157
Fort Worth, TX 76161-0057

David T. Rankin
Senior General Attorney
BNSF Railway Company
2500 Lou Menk Drive, AOB-3
Fort Worth, Texas 76131-2828

Service Date: May 11, 2011

ENVIRONMENTAL REPORT

(49 C.F.R. § 1105.7)

*(1) **Proposed Action and Alternatives.** Describe the proposed action, including commodities transported, the planned disposition (if any) of any rail line and other structures that may be involved, and any possible changes in current operations or maintenance practices. Also describe any reasonable alternatives to the proposed action. Include a readable, detailed map and drawings clearly delineating the project.*

BNSF Railway Company (“BNSF”) proposes to abandon its Rail Freight Service Easement over the 4.85 miles of rail line located between Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and Milepost 119.35, just east of the San Gabriel River, in Irwindale, in Los Angeles County, California (the “Line”). A map of the project area is attached as **Exhibit A**.

The physical assets of the Line are owned by the Los Angeles County Metropolitan Transportation Authority (“LACTMA”). LACTMA desires to extend its current light rail service eastward to Azusa, CA. The removal of the track and track materials associated with the abandonment of BNSF’s Rail Freight Service Easement and the extension of the light rail service have already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report (“SEIR”). The salvaging of the Line will be conducted by LACTMA consistent with the mitigation measures set forth in the SEIR. A copy of the Executive Summary of the SEIR is attached as **Exhibit B**. The entire reports can be viewed at the following web link:

http://www.foothillextension.org/construction_phases/phase_2a_pasadena_to_azusa/supplemental-environmental-impact-report/2010-draft-supplemental-environmental-impact-report/

The Line has had no local traffic for some time. The Line is stub-ended and does not handle overhead traffic. The Line was used for storing empty freight cars which will now be stored in other locations as needed. Therefore, there will be no change to any freight service provided on the Line. Because of the lack of traffic on the Line, only very limited maintenance has been performed on the Line for some time. The only alternative to abandonment would be to not abandon the Line and jeopardize LACMTA's desires to extend its current light rail service eastward.

(2) Transportation System Describe the effect of the proposed action on regional or local transportation systems and patterns. Estimate the amount of traffic (passenger or freight) that will be diverted to other transportation systems or modes as a result of the proposed action.

There will be no passenger or freight traffic diverted to other transportation systems as a result of the proposed abandonment. There has been no local or overhead traffic on this line for some time.

(3) Land Use

(i) Based on consultation with local and/or regional planning agencies and/or review of the official planning documents prepared by such agencies, state whether the proposed action is consistent with existing land use plans. Describe any inconsistencies.

The proposed action is consistent with existing land use plans. See SEIR. BNSF contacted the City of Arcadia, Planning Commission, the City of Irwindale, Planning Commission, and the County of Los Angeles, Department of Regional Planning.

As of the date of this report we have not received any replies from these agencies.

Copies of the letters are attached as **Exhibit C**.

(ii) Based on consultation with the U.S. Soil Conservation Service, state the effect of the proposed action on any prime agriculture land.

Proposed abandonment will not have an adverse effect on prime agriculture land.

See SEIR. BNSF sent a letter to the California NRCS State Office, dated April 29, 2011, and as of the date of this report we have not received a reply. A copy of the letter is attached as **Exhibit D**.

(iii) If any action affects land or water uses within a designated coastal zone, include the coastal zone information required by § 1105.9.

Not applicable.

(iv) If the proposed action is an abandonment, state whether or not the right-of-way is suitable for alternative public use under 49 U.S.C. § 10905 and explain why.

The proposed abandonment of BNSF's Rail Freight Service Easement will facilitate LACMTA's desires to extend its current light rail service eastward. BNSF contacted the City of Arcadia, Planning Commission, the City of Irwindale, Planning Commission, and the County of Los Angeles, Department of Regional Planning and as of the date of this report has not received a reply regarding any alternative public use of the rail line. Copies of the letters are attached as **Exhibit C**.

(4) Energy

(i) Describe the effect of the proposed action on transportation of energy resources.

The proposed abandonment will have no effect on the transportation of energy resources.

(ii) Describe the effect of the proposed action on recyclable commodities.

The proposed abandonment will not adversely affect the movement or recovery of recyclable commodities.

(iii) State whether the proposed action will result in an increase or decrease in overall energy efficiency and explain why.

The proposed action will not result in an increase or decrease in overall energy efficiency as there has been no traffic on the line for more than two years.

(iv) If the proposed action will cause diversions from rail to motor carriage of more than:

(A) 1,000 rail carloads a year, or

(B) an average of 50 rail carloads per mile per year for any part of the affected line, quantify the resulting net change in the energy consumption and show the data and methodology used to arrive at the figure given.

The proposed abandonment will not result in a diversion of rail to motor carriage.

(5) Air

(i) If the proposed action will result in either:

(A) an increase in rail traffic of at least 100 percent (measured in gross ton miles annually) or an increase of at least eight trains a day on any segment of the line affected by the proposal, or

(B) an increase in rail yard activity of at least 100 percent (measured by carload activity), or

(C) an average increase in truck traffic of more than 10 percent of the average daily traffic or 50 vehicles a day on any affected road segment, quantify the anticipated effect on air emissions.

The proposed action will not result in meeting or exceeding the specified thresholds for increased rail or truck traffic as outlined in (i) (A), (B) or (C) above.

(ii) If the proposed action affects a class I or nonattainment area under the Clean Air Act, and will result in either:

(A) an increase in rail traffic of at least 50 percent (measured in gross ton miles annually) or an increase of at least three trains a day on any segment of rail line,

(B) an increase in rail yard activity of at least 20 percent (measured by carload activity), or

(C) an average increase in truck traffic of more than 10 percent of the average daily traffic or 50 vehicles a day on a given road segment, then state whether any expected increased emissions are within the parameters established by State Implementation Plan. However, for a rail construction under 49 U.S.C. § 10901 (or 49 U.S.C. § 10505) or a case involving the reinstatement of service over a previously abandoned line, only the three train a day threshold in this item shall apply.

The proposed action will not result in meeting or exceeding the specified thresholds in (ii) (A), (B) or (C) above.

(iii) If the transportation of ozone depleting materials (such as nitrogen oxide and Freon) is contemplated, identify: the materials and quantity; the frequency of service; safety practices (including any speed restrictions); the applicant's safety record (to the extent available) on derailments, accidents and spills; contingency plans to deal with accidental spills; and the likelihood of an accidental release of ozone depleting materials in the event of a collision or derailment.

The proposed abandonment will not affect the transportation of ozone depleting materials.

(6) Noise If any of the thresholds identified in item (5) (i) of this section are surpassed, state whether the proposed action will cause:

(i) an incremental increase in noise levels of three decibels Ldn or more; or

(ii) an increase to a noise level of 65 decibels Ldn or greater. If so, identify sensitive receptors (e.g. schools, libraries, hospitals, residences, retirement communities and nursing homes) in the project area and quantify the noise increase for these receptors if the thresholds are surpassed.

Not applicable.

(7) **Safety**

(i) Describe any effects of the proposed action on public health and safety (including vehicle delay time at railroad crossings).

This abandonment should have no adverse effect on health or public safety.

(ii) If hazardous materials are expected to be transported, identify: the materials and quantity; the frequency of service; whether chemicals are being transported that, if mixed, could react to form more hazardous compounds; safety practices (including any speed restrictions); the applicant's safety record (to the extent available) on derailments, accidents and hazardous spills; the contingency plans to deal with accidental spills, and the likelihood of and accidental release of hazardous materials.

The abandonment will not result in the transportation of hazardous materials.

(iii) If there are any known hazardous waste sites or sites where there have been known hazardous material spills on the right-of-way, identify the location of those sites and the types of hazardous materials involved.

There are no known hazardous waste sites or sites where there have been known hazardous material spills on the right-of-way.

(8) **Biological Resources**

(i) Based on consultation with the U.S. Fish and Wildlife Service, state whether the proposed action is likely to adversely affect endangered or threatened species or areas designated as a critical habitat, and if so, describe the effects.

The proposed abandonment will not have an adverse effect on endangered or threatened species or areas designated as a critical habitat. See SEIR. By letter dated April 29, 2011, BNSF contacted the U.S. Fish and Wildlife Service, Sacramento Fish & Wildlife Office, in reference to this proposed abandonment. As of the date of this Environmental Report, the agency has not responded to our inquiry. A copy of the letter is attached as **Exhibit E**.

(ii) State whether wildlife sanctuaries or refuges, National or State parks or forests will be affected, and describe any effects.

BNSF does not believe that any wildlife sanctuaries or refuges, National or State parks or forests will be adversely affected by the proposed abandonment. See SEIR. By letters dated April 29, 2011, BNSF contacted the U.S. Department of the Interior, Bureau of Land Management (California State Office), and the U.S. Department of the Interior, National Park Service in reference to the proposed abandonment. As of the date of this Environmental Report, neither agency has responded to our inquiries. Copies of the letters are attached as **Exhibit F**.

(9) **Water**

(i) Based on consultation with State water quality officials, state whether the proposed action is consistent with applicable Federal, State or local water quality standards. Describe any inconsistencies.

By letters dated April 29, 2011, BNSF contacted the U.S. EPA Region 9, and the California Department of Water Resources in reference to the proposed abandonment. As of the date of this report, neither agency has responded to our inquiry. Copies of the letters are attached as **Exhibit G**.

(ii) Based on consultation with the U.S. Army Corps of Engineers, state whether permits under Section 404 of the Clean Water Act (33 U.S.C. § 1344) are required for the proposed action and whether any designated wetlands or 100-year flood plains will be affected. Describe the effects.

No designated wetlands or 100-year flood plains will be adversely affected by the proposed abandonment. See SEIR. By letter dated April 29, 2011, BNSF contacted the Los Angeles District of the U.S. Army Corps of Engineers in reference to the proposed abandonment. As of the date of this Environmental Report, the Corps has not responded to our inquiry. A copy of the letter is attached as **Exhibit H**.

(iii) State whether permits under Section 402 of the Clean Water Act (33 U.S.C. § 1342) are required for the proposed action. (Applicants should contact the U.S. Environmental Protection Agency or the state environmental protection or equivalent agency if they are unsure whether such permits are required).

By letters dated April 29, 2011, BNSF contacted the U.S. EPA Region 9, and the California Department of Water Resources in reference to the proposed abandonment. As of the date of this report, neither agency has responded to our inquiry. Copies of the letters are attached as **Exhibit G**.

(10) Proposed Mitigation. Describe any actions that are proposed to mitigate adverse environmental impacts, indicating why the proposed mitigation is appropriate.

BNSF does not expect any adverse environmental impact from the proposed abandonment and, therefore, sees no need for any mitigating actions. BNSF will, of course, consult (as required) with any recipients of this Environmental Report regarding appropriate mitigation actions and will comply with those mitigation actions required by the Board.

**BEFORE THE
SURFACE TRANSPORTATION BOARD**

| | | |
|-------------------------------|---|------------------------|
| BNSF RAILWAY COMPANY |) | |
| ABANDONMENT EXEMPTION |) | DOCKET NO. AB-6 |
| IN LOS ANGELES COUNTY, |) | (SUB-NO. 477X) |
| CALIFORNIA | | |

HISTORIC REPORT

BNSF RAILWAY COMPANY
2650 Lou Menk Drive
P.O. Box 96157
Fort Worth, TX 76161-0057

David T. Rankin
Senior General Attorney
BNSF Railway Company
2500 Lou Menk Drive, AOB-3
Fort Worth, Texas 76131

Service Date: May 11, 2011

HISTORIC REPORT

(49 C.F.R. § 1105.8)

*(1) **Proposed Action and Alternatives.** Describe the proposed action, including commodities transported, the planned disposition (if any) of any rail line and other structures that may be involved, and any possible changes in current operations or maintenance practices. Also describe any reasonable alternatives to the proposed action. Include a readable, detailed map and drawings clearly delineating the project.*

BNSF Railway Company (“BNSF”) proposes to abandon its Rail Freight Service Easement over the 4.85 miles of rail line located between Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and Milepost 119.35, just east of the San Gabriel River, in Irwindale, in Los Angeles County, California (the “Line”). A map of the project area is attached as **Exhibit A**.

The physical assets of the Line are owned by the Los Angeles County Metropolitan Transportation Authority (“LACTMA”). LACTMA desires to extend its current light rail service eastward to Azusa, CA. The removal of the track and track materials associated with the abandonment of BNSF’s Rail Freight Service Easement and the extension of the light rail service have already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report (“SEIR”). The salvaging of the Line will be conducted by LACTMA consistent with the mitigation measures set forth in the SEIR. A copy of the Executive Summary of the SEIR is attached as **Exhibit B**. Also, a copy of the Supplemental Historic Properties Survey and Effects Report (dated September, 2005) is attached as **Exhibit I**. The entire reports can be viewed at the following web link:

http://www.foothillextension.org/construction_phases/phase_2a_pasadena_to_azusa/supplemental-environmental-impact-report/2010-draft-supplemental-environmental-impact-report/

The Line has had no local traffic for some time. The Line is stub-ended and does not handle overhead traffic. The Line was used for storing empty freight cars which will now be stored in other locations as needed. Therefore, there will be no change to any freight service provided on the Line. Because of the lack of traffic on the Line, only very limited maintenance has been performed on the Line for some time.

The only alternative to abandonment would be to not abandon the Line and jeopardize LACMTA's desires to extend its current light rail service eastward.

HISTORIC REPORT

- 1. A U.S.G.S. topographic map (or an alternate map drawn to scale and sufficiently detailed to show buildings and other structures in the vicinity of the proposed action) showing the location of the proposed action, and the locations and approximate dimensions of railroad structures that are 50 years old or older and are part of the proposed action.*

The required topographic map is attached to this Report as **Exhibit A**.

- 2. A written description of the right-of-way (including approximate widths, to the extent known), and the topography and urban and/or rural characteristics of the surrounding area*

The subject Line extends approximately 4.85 miles between Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and Milepost 119.35, just east of the San Gabriel River, in Irwindale, in Los Angeles County, California. The average width of the right-of-way is generally 100 feet, 50 feet on each side of the centerline of the Line. The width decreases on some segments between 50 feet to 75 feet on each side of the centerline and then changes back to 100 feet wide. There are federally granted rights of way involved.

- 3. Good quality photographs (actual photographic prints, not photocopies) of railroad structures on the property that are 50 years old or older and of the immediately surrounding area.***

There are six bridges on the Line. See attached **Exhibit J**.

- 4. The date(s) of construction of the structure(s), and the date(s) and extent of any major alterations, to the extent such information is known.***

There are six bridges on the Line. See attached **Exhibit J**.

- 5. A brief narrative history of carrier operations in the area, and an explanation of what, if any, changes are contemplated as a result of the proposed action.***

On September 5, 1883, the Los Angeles and San Gabriel Valley Rail Road Company ("LASGV") was incorporated in California. On May 20, 1887, LASGV was consolidated into California Central Railway Company ("CCR"). On November 7, 1889, CCR was consolidated into Southern California Railway Company ("SCR"). On January 17, 1906, SCR property was sold to The Atchison, Topeka and Santa Fe Railway Company ("ATSF"). In 1996, the Burlington Northern Railroad Company merged with ATSF to become The Burlington Northern and Santa Fe Railway Company, which name was changed to BNSF Railway Company in 2005.

- 6. A brief summary of documents in the carrier's possession, such as engineering drawings, that might be useful in documenting a structure that is found to be historic.***

Documents in BNSF's possession concerning this abandonment may include alignment maps showing the right-of-way and/or station maps. These documents are too large for practical reproduction in this report, but can be furnished upon request, if they are available.

- 7. An opinion (based on readily available information in the railroad's possession) as to whether the site and/or structures meet the criteria for listing on the National Register of Historic Places (36 CFR 60.4), and whether there is a likelihood of archeological resources or any other previously unknown historic properties in the project area, and***

the basis for these opinions (including any consultations with the State Historic Preservation Office, local historical societies or universities).

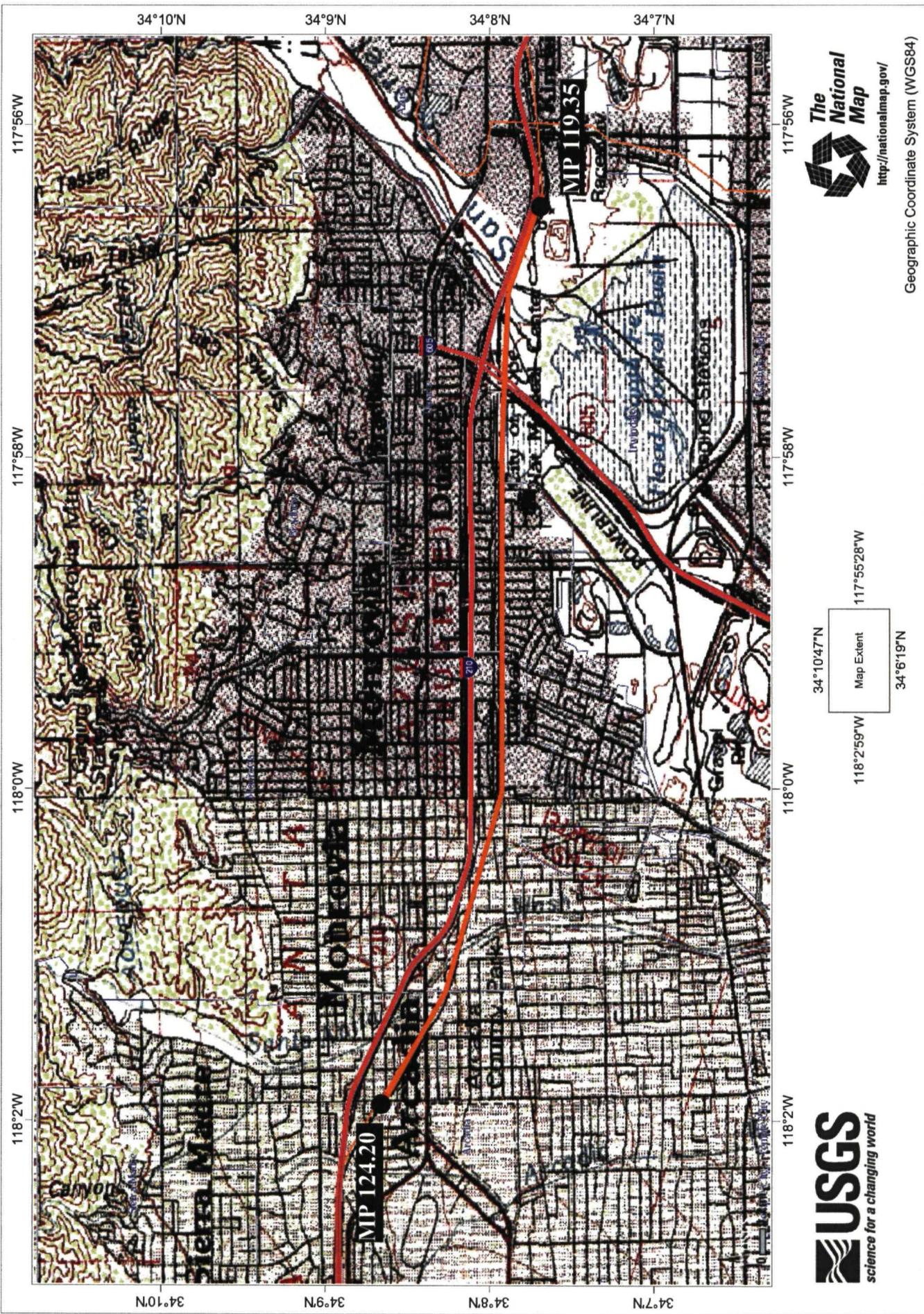
By letter dated April 29, 2011, BNSF contacted the Office of Historic Preservation, California State Parks (“SHPO”) in reference to the proposed abandonment and as of the date of this report has not received a reply. A copy of the letter is attached as **Exhibit K**.

- 8. A description (based on readily available information in the railroad's possession) of any known prior subsurface ground disturbance or fill, environmental conditions (naturally occurring or manmade) that might affect the archeological recovery of resources (such as swampy conditions or the presence of toxic wastes), and the surrounding terrain.***

The Line was disturbed during original construction by cuts and fill and any archaeological resources that may have been located in the proposed project area would have been affected at that time. Our records do not indicate any environmental conditions that might affect the archaeological recovery of resources.

- 9. Within 30 days of receipt of the historic report, the State Historic Preservation Officer may request the following additional information regarding specific non railroad owned properties or groups of properties immediately adjacent to the railroad right-of-way: photographs of specified properties that can be readily seen from the railroad right-of-way (or other public rights-of-way adjacent to the property) and a written description of any previously discovered archeological sites, identifying the location and type of the site (i.e. prehistoric or native American).***

If any additional information is requested, BNSF will promptly supply the necessary information.



34°10'N 34°9'N 34°8'N 34°7'N

117°56'W 117°58'W 118°0'W 118°02'W

118°2'W 118°2'59'W 34°10'47'N 117°55'28'W

117°56'W 117°58'W 118°0'W 118°02'W

118°2'W 118°2'59'W 34°10'47'N 117°55'28'W

34°10'N 34°9'N 34°8'N 34°7'N



<http://nationalmap.gov/>

Geographic Coordinate System (WGS84)

34°10'47'N
117°55'28'W
Map Extent
118°2'59'W
34°6'19'N



Gold Line Phase II

Pasadena to Montclair - Foothill Extension 2A

Final Supplemental Environmental Impact Report (SEIR)

(SCH No. 2003061157)
December 2010

Executive Summary: SEIR



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Chapter 1. Executive Summary

1.1 Background

The Metro Gold Line Foothill Extension Construction Authority (the “Authority”) previously prepared a Draft Environmental Impact Statement/Environmental Impact Report (Draft EIS/EIR) for the Gold Line Foothill Extension Project (the “Project”). The Gold Line Foothill Extension is referred to as Phase 2 of the overall Gold Line Foothill Extension Project and, at complete build out, would span from the cities of Pasadena to Montclair. The Foothill Extension Project was divided into two subsequent phases: Phase 2A, spanning from Pasadena to Azusa, and Phase 2B, spanning from Azusa to Montclair. In conjunction with the Authority’s decision to proceed with Phase 2A, a Final EIR was prepared based on the Draft EIS/EIR and was certified in 2007, though only for the purposes of extension’s Phase 2A. Because no federal action or funding is involved in Phase 2A, no NEPA action is required. The portion of the Phase 2A from Pasadena to Azusa includes 11.5 miles of track through six cities (Pasadena, Arcadia, Monrovia, Duarte, Irwindale, and Azusa), six stations, and the construction of a new Maintenance and Operation Facility (M&O Facility).

The *Gold Line Phase II Pasadena to Montclair-Foothill Extension Final Environmental Impact Report* (2007 Final EIR) previously analyzed the Maintenance and Operations Facility (M&O Facility) at a different location, and construction was planned to be part of Phase 2B. However, current planning calls for completion and operation of the M&O Facility as part of Phase 2A, with a potential site in the City of Monrovia having been identified. Nonetheless, the previously identified Coors-Miller Brewery Company site in Irwindale is analyzed as an alternative to the Monrovia site, as part of the alternatives analysis required by CEQA.

In addition to the M&O Facility relocation, other Phase 2A refinements have been identified after certification of the 2007 Final EIR. These include the realignment of Mountain Avenue at Duarte Road, the relocation of parking structures at the Monrovia and Irwindale LRT stations, replacement of the North Colorado Boulevard Bridge, and the replacement of the San Gabriel River Bridge. Since only minor additions or changes to the 2007 Final EIR would occur as a result of the proposed project revisions, a Supplemental Environmental Impact Report (SEIR) to the 2007 Final EIR will be prepared to provide environmental clearance for the M&O Facility and other Phase 2A refinements.

1.2 Study Area Refinements

All proposed construction and improvements would occur within the Pasadena to Azusa portion of the Project. The 2007 Final EIR included analysis of an M&O Facility in the City of Irwindale adjacent to and west of the Miller-Coors Brewing Company facility. Since publication of the 2007 Final EIR, a new site has been identified. The M&O Facility would now be located in the City of Monrovia on a 27-acre tract of land located south of East Evergreen Avenue (frontage road to I-210 freeway), west of Shamrock Avenue, north of Duarte Road, and east of South California Avenue. At present, two options (Option A and B) for the M&O Facility design are being considered for the Monrovia site. Option A would occupy 27 acres, whereas Option B would occupy 24 acres. The



primary difference between the two options is that Option B would not require a 3.0 acre tract of land located in the southeast portion of the study area, which is currently put to an industrial use, and would reduce project costs. Although the 2007 Final EIR included analysis of parking for each LRT station, relocated parking facilities are currently being proposed at the Monrovia and Irwindale LRT stations due to site constraints at the previously identified locations. The realignment of Mountain Avenue at Duarte Road, the replacement of the North Colorado Boulevard Bridge, and replacement of the San Gabriel River Bridge are also proposed due to design constraints discovered since the 2007 Final EIR was certified. Each of these additional refinements will be analyzed accordingly in this draft SEIR.

1.3 Project Objectives

This project is being developed by the Authority to support operations of the Metro Gold Line and other light rail transit systems. Specific objectives of the project include:

M&O Facility refinements:

- Develop a maintenance and operations facility yard to accommodate LRT system capacity and storage requirements,
- Provide facilities to perform routine and special maintenance for Light Rail Vehicles (LRVs),
- Provide facilities to perform light and heavy duty LRV fleet repairs, and
- Provide storage facilities for LRVs including facilities to house the trains overnight.

Other refinements:

- Realign the Mountain Ave./Duarte Rd. intersection to improve safety,
- Relocate parking at Monrovia Station to better accommodate the City of Monrovia's future transit oriented development (TOD),
- Relocate parking location and configuration at Irwindale station and improve safety and constructability at the Irwindale Station,
- Replace the Colorado Boulevard Bridge to address structural issues and minimize property requirements, and
- Replace the San Gabriel River Bridge design.

Furthermore, the Authority strives for the M&O Facility in Monrovia to be designed and constructed to meet Leadership in Energy and Environmental Design (LEED[®]) Silver Certification. The LEED[®] certification program encourages and accelerates global adoption of sustainable "green" buildings and development practices, recognizing projects that implement strategies for better environmental and health performance. As such, these facilities will be constructed to minimize environmental impacts.



1.4 Project Alternatives Discussion

A number of alternatives were initially evaluated during the Alternatives Analysis portion of studies conducted for the Gold Line Foothill Extension (Gold Line Phase II Extension Pasadena to Claremont Alternatives Analysis, Final Draft Report, dated January 9, 2003). The alternatives analysis for the Gold Line Foothill Extension Phase 2 is described in detail in the 2007 Final EIR.

The 2007 Final EIR previously analyzed the M&O Facility at a different location (City of Irwindale, Miller-Coors Brewery property), and construction was planned to be part of Phase 2B. However, current planning calls for completion and operation of the M&O Facility as part of Phase 2A, with a potential site in the City of Monrovia having been identified. Nonetheless, the previously identified Miller-Coors Brewery Company Irwindale site is analyzed as an alternative to the Monrovia site, as part of the alternatives analysis required by CEQA. This site is described below and is referred to as M&O Facility in Irwindale (Alternative 2).

Similar to the proposed M&O Facility in Monrovia (Option A and B), Alternative 2 would support operations of the Metro Gold Line and other light rail transit systems. The M&O Facility in Irwindale (Alternative 2) is described in the following section and is shown in Figure 5-1. This alternative was analyzed in the 2007 Final EIR and for the purposes of the Draft SEIR for the Foothill Extension Phase 2A Project refinements and is being evaluated as an alternative to the proposed M&O Facility in Monrovia, as described in Chapter 3 Project Description.

1.5 Environmental Process

The SEIR was prepared following input from affected agencies and members of the public. In accordance with Section 15063.15082 of the CEQA Guidelines, a notice of preparation (NOP) was prepared and distributed to responsible and affected agencies and other interested parties for public review. The public review period for the NOP began on May 17, 2010, and ended on July 2, 2010. The NOP was also posted in the Authority's office and sent to the State Clearinghouse at the Governor's Office of Planning and Research to officially solicit statewide agency participation in determining the scope of the SEIR (SCH# 2003061157). In addition, the NOP was sent to agencies along the Phase 2A corridor. The NOP also included a project description as well as description of alternatives and potential environmental impacts. A public notice was published in the *San Gabriel Valley Tribune* on June 9, 2010, providing details of scoping meetings held on June 16 and June 17, 2010 in Monrovia and Irwindale, respectively, and soliciting comments on the scope of the SEIR. Additionally, postcards were sent to property owners within the vicinity of the proposed Project refinements to notify them of the Project and the scoping meetings. Written comments submitted at the scoping meeting are contained in Appendix A.

The Draft SEIR ~~was being~~ distributed directly to numerous agencies, organizations, and interested groups and persons for formal comment during the review period. The Draft SEIR ~~was~~ is also available for review online at http://www.metrogoldline.org/SEIR_page.html and at the following locations:

- Metro Gold Line Foothill Extension Construction Authority: 406 East Huntington Drive, Suite 202, Monrovia, California 91016



- City of Monrovia, Planning Division: 415 South Ivy Avenue, Monrovia, CA 91016
- City of Arcadia Planning Department: 240 W. Huntington Drive Arcadia, CA 91007
- City of Irwindale Planning Department: 5050 North Irwindale Avenue Irwindale, CA 91706
- City of Duarte Planning Department: 1600 Huntington Drive Duarte, CA 91010

The Authority ~~will~~ received public input on the Project and the SEIR at a hearing on October 27, 2010 at 4:00 PM.

This SEIR ~~was~~ ~~is being~~ circulated for public review and comment for a period of ~~7845~~ days. During this period, comments on environmental issues raised in the SEIR, ~~and~~ along with the SEIR's accuracy and completeness ~~were~~ may be submitted to the lead agency at the following address:

Metro Gold Line Foothill Extension Construction Authority
ATTN: Lisa Levy Buch, Director of Public Affairs
406 East Huntington Drive, Suite 202, Monrovia, California 91016

Formal comments on the SEIR ~~were~~ ~~must be~~ submitted and delivered to the address above by 5 pm on the last day of the public review period identified in the Notice of Availability. Upon completion of the public review period, a final SEIR ~~was~~ will be prepared that ~~will~~ included the comments on the Draft SEIR received during the formal public review period as well as responses to those comments and revisions to the Draft SEIR, if any, that are necessary to address issues raised in the comments.

Prior to approval of the proposed ~~P~~project, the Authority ~~will~~ considered whether to certify that the SEIR has been completed in compliance with CEQA, that the Authority has reviewed and considered the information in the SEIR, and that the SEIR reflects the independent judgment of the Authority.

1.6 Summary of Impacts and Mitigation by Refinement

The below table summarizes the impacts and associated mitigations, where applicable, for the proposed project refinements.



Table 1-1: Summary of Impacts and Mitigation by Refinement

| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|---|---|---|
| <p>AESTHETICS</p> | <p>Mitigation Measures V-1 from the 2007 Final EIR and V-3 would be applicable. V-2 from the 2007 Final EIR would not be applicable.</p> <p>V-3: As an extension of V-1 in the 2007 Final EIR, the proposed mitigation for the removal of the hedgerow in the Authority's right-of-way along Duarte would be to provide landscaping in a manner consistent with the landscape treatments used in Phase I of the Project. See Section 4.1.5 for full text.</p> | <p>With implementation of Mitigation Measures V-1, V-3, V-4, CR-4, and CR-5, aesthetic impacts would be reduced to a less than significant level.</p> |
| <p>Visual Effects - M&O Facility: Significant impact</p> | <p>Mitigation Measures V-4, CR-4, and CR-5 would be applicable.</p> <p>V-4: The proposed dual track bridge, which will replace the existing single-track bridge at the North Colorado Boulevard overcrossing, shall conform to all applicable Metro design criteria and include aesthetic treatment to be determined by the Authority in coordination with the City of Arcadia and a qualified bridge architect and/or architectural historian during final design. The newly constructed dual-track bridge, which will replace the existing single-track bridge at the North Colorado Boulevard overcrossing, will include aesthetic treatments to be determined by the Authority in coordination with a qualified landscape architect during final design. See Section 4.1.5 for full text.</p> | <p>Impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Visual Effects - North Colorado Boulevard Bridge Replacement: Significant impact</p> | <p>N/A</p> | <p>All land use and planning impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Mountain Avenue Realignment: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Monrovia LRT Station Parking Structure: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Inwindale LRT Station Parking Lot/Structure: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>LAND USE</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Conflicts with habitat or natural community conservation plans: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Division of established communities: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Conflicts with jurisdictional land use plans, policies, or regulations that have been adopted for the purpose of avoiding or mitigating environmental effects: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Construction period impacts: No significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>POPULATION & HOUSING</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Population and housing: Less than significant impact</p> | <p>N/A</p> | <p>All population and housing impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |



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| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|--|--|--|
| TRANSPORTATION & TRAFFIC | | |
| M&O Facility - Construction-period impacts: Significant impact | <p>Mitigation Measures T-1 through T-6 from the 2007 Final EIR and T-7 would be applicable. See Section 4.4.5 for full text.</p> <p>T-7 The impact at the intersection of California Avenue/Evergreen Avenue would be removed, once additional capacity is restored at completion of current I-210 freeway construction. To mitigate the construction-period impact at the intersection of Shamrock Avenue/Evergreen Avenue, truck routes that use this intersection be restricted to off-peak periods only.</p> | <p>With implementation of Mitigation Measures T-1 through T-7, transportation & traffic impacts would be reduced to a less than significant level.</p> |
| Mountain Avenue Realignment: No significant impact. | N/A | Impacts would be less than significant. |
| Monrovia LRT Station Parking Structure: Significant impact | Mitigation Measures T-5 and T-6 from the 2007 Final EIR would be applicable. | Therefore, no mitigation measures would be required. |
| Inwindale Station LRT Parking Lot/Structure: No significant impact | N/A | |
| North Colorado Boulevard Bridge Replacement - Construction-period impacts: Significant impact | Mitigation Measures T-1 through T-6 from the 2007 Final EIR and T-7 would be applicable. See Section 4.4.5 for full text. | With implementation of Mitigation Measures T-1 through T-6, transportation & traffic impacts would be reduced to a less than significant level. |
| CULTURAL RESOURCES | | |
| Cause a substantial adverse change in the significance of a historical resource: No impact | N/A | Impacts would be less than significant. |
| Cause a substantial adverse change in the significance of an archaeological resource: No impact | N/A | Therefore, no mitigation measures would be required. |
| Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature: No impact | N/A | |
| Yield, or may be likely to yield, information important in prehistory or history: Potentially significant impact | <p>Mitigation Measures CR-1 through CR-3 from the 2007 Final EIR as well as CR-4 and CR-5 would be applicable. See Section 4.5.5 for full text.</p> <p>CR-4 A comprehensive documentation program shall be completed on the existing bridge prior to the commencement of the proposed project (North Colorado Boulevard Bridge Replacement refinement only).</p> <p>CR-5 The replacement bridge to be constructed at the site during this project shall incorporate, as appropriate, the Art Deco-style motifs on the existing bridge, while clearly distinguishing itself from similar bridges of historic origin to avoid any future confusion. The work shall be coordinated with the Authority and the City of Arcadia, as well as with a qualified bridge architect or architectural historian. (North Colorado Boulevard Bridge Replacement refinement only)</p> | <p>With implementation of Mitigation Measures CR-1 through CR-5, cultural resources impacts would be reduced to a less than significant level.</p> |

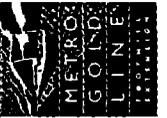


| Environmental Impacts: | Mitigation Measures: | Impact Results with Mitigation |
|--|--|---|
| <p>HAZARDS & HAZARDOUS MATERIALS</p> <p>Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials: Potentially significant impact</p> | <p>Mitigation Measures HZ-1 through HZ-10 from the 2007 Final EIR as well as HZ-11, HZ-12, and HZ-13 would be applicable. See Section 4.6.5 for full text.</p> <p>HZ-11 Prior to issuing grading permits for the Monrovia LRT Station Parking and Monrovia M&O Facility sites, the Phase 2 ESA currently being prepared, the Removal Action Completion Reports currently under review by regulatory agencies, and environmental assessments being managed by the City of Monrovia shall be implemented, along with any additional recommendations for remedial action contained in these reports.</p> <p>HZ-12 Prior to issuing a grading permit for the M&O Facility and Monrovia LRT Station Parking Structure sites, a health and safety plan shall be developed for persons with a potential for exposure to the constituents of concern.</p> <p>HZ-13 During construction activities, the contractor shall immediately notify the appropriate local authority if any unknown substances, subsurface tank/piping or potentially hazardous materials are encountered.</p> | <p>With implementation of Mitigation Measures HZ-1 through HZ-13 and compliance with federal, state, and other applicable regulatory requirements, hazards & hazardous materials impacts would be reduced to a less than significant level.</p> |
| <p>Release of hazardous materials into the environment: Less than significant</p> <p>Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school: Less than significant impact</p> | <p>N/A</p> | <p>Impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5: Potentially significant impact</p> | <p>Mitigation Measures HZ-1 through HZ-10 from the 2007 Final EIR as well as HZ-11, HZ-12, and HZ-13 would be applicable (see above).</p> | <p>With implementation of Mitigation Measures HZ-1 through HZ-13 and compliance with federal, state, and other applicable regulatory requirements, hazards & hazardous materials impacts would be reduced to a less than significant level.</p> |
| <p>For a project located within an airport land use plan within 2 miles of a public airport or public use airport: No impact</p> <p>For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area: No impact</p> | <p>N/A</p> | <p>Impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan: No impact</p> <p>Interference with existing emergency response plans or emergency evacuation plans: No impact</p> <p>Expose people or structures to a significant risk of loss, injury, or death involving wildland fires: No impact</p> | <p>N/A</p> | <p>Impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |



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| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|--|--|--|
| PUBLIC SERVICES & FACILITIES | | |
| Public services and facilities: Less than significant impact | N/A | All public services and facilities impacts would be less than significant. Therefore, no mitigations measures are necessary. |
| UTILITIES / SERVICE SYSTEMS | | |
| Exceed wastewater treatment requirements of the applicable RWQCB: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| Requires the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects: Potentially significant impact | Mitigation Measures U-1 through U-6 from the 2007 Final EIR as well as U-8 would be applicable. U-8 The Authority shall consult with the County, cities, and regional agencies related to water supply and the Urban Water Management Plan to ensure that operation of the proposed Project refinements will not conflict with water supply agreements and conditions, or result in the need for construction of expanded or new water supply facilities. The Authority will also minimize solid waste generated during construction through the recycling of building materials. | With implementation of the Mitigation Measures U-1 through U-6 and U-8, utilities/service systems impacts would be reduced to a less than significant level. |
| Requires the construction of new stormwater drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| There is sufficient water supplies available to serve the Project refinement from existing entitlements and resources, or new or expanded entitlements are needed: Potentially significant impact | Mitigation Measures U-1 through U-6 from the 2007 Final EIR as well as U-8 would be applicable. U-8 The Authority shall consult with the County, cities, and regional agencies related to water supply and the Urban Water Management Plan to ensure that operation of the proposed Project refinements will not conflict with water supply agreements and conditions, or result in the need for construction of expanded or new water supply facilities. The Authority will also minimize solid waste generated during construction through the recycling of building materials. | With implementation of the Mitigation Measures U-1 through U-6 and U-8, utilities/service systems impacts would be reduced to a less than significant level. |
| Results in a determination by the wastewater treatment provider, which serves or may serve the project, that it does not have adequate capacity to serve the project refinements projected demand in addition to the provider's existing commitments: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |

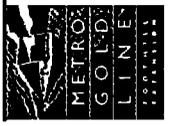


| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|---|---|--|
| <p>Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs: Significant impact</p> | <p>Mitigation Measures U-1 through U-6 from the 2007 Final EIR as well as U-7 would be applicable (see above). U-7 Construction Period Solid Waste Impacts. The Authority shall consult with the County or private waste management companies to reduce construction waste through construction and demolition reuse and recycling programs. The Authority will also minimize solid waste generated during construction through the recycling of building materials.</p> | <p>With implementation of the Mitigation Measures U-1 through U-7, utilities/service systems impacts would be reduced to a less than significant level.</p> |
| <p>Comply with federal, state, and local statutes and regulations related to solid waste: Significant impact</p> | <p>Mitigation Measures U-1 through U-6 from the 2007 Final EIR as well as U-7 and U-8 would be applicable (see above).</p> | <p>With implementation of the Mitigation Measures U-1 through U-8, utilities/service systems impacts would be reduced to a less than significant level.</p> |
| AIR QUALITY & GREENHOUSE GAS EMISSIONS | | |
| <p>Short-term construction air quality impacts: Significant impact</p> | <p>Mitigation Measures A-1 through A-12 as well as A-13 and A-14 would be applicable. A-13 Painting restrictions for the M&O Facility shall include: <ul style="list-style-type: none"> • Limit the amount of painting each day, spreading the amount being painted evenly over a one month period (or longer). • No painting of the exterior surfaces would occur. Exterior surfaces would utilize pre-coated, pre-colored, naturally colored, factory painted materials. • Low-VOC paints would be used for all painted surfaces • Up to 75% of Building B-02 would be painted, and up to 10% of the interior surfaces in total would be painted for the remaining building. A-14 Watering of exposed areas shall occur a minimum of three times daily during grading operations in a manner consistent with the SCAQMD Rules and Regulations.</p> | <p>With implementation of the Mitigation Measures A-1 through A-14, air quality & green house gas emissions impacts would be reduced to a less than significant level.</p> |
| <p>Long-term regional air quality impacts: No significant impact</p> | | |
| <p>Paint & odors: No significant impact</p> | | |
| <p>Traffic: No significant impact</p> | | |
| <p>Toxic air contaminants: No significant impact</p> | | |
| <p>Compliance with air quality planning: No impact</p> | | |
| <p>Projected GHG emissions: No significant impact</p> | N/A | <p>Impacts would be less than significant. Therefore, no mitigation measures would be required.</p> |



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| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|---|---|--|
| GEOLOGY & SOILS | | |
| Expose people or structures to potential substantial adverse effect, including the risk of loss, injury, or death involving rupture of a known earthquake fault: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| Seismic ground shaking: Potentially significant impact | Mitigation Measure GS-1 would be applicable. GS-1 California Building Code Compliance and Seismic Standards. Prior to grading or building, the Authority shall obtain a soils engineering report(s) prepared by a qualified soils engineer. | With implementation of the Mitigation Measure GS-1, geology & soils impacts would be reduced to a less than significant level. |
| Seismic-related ground failure, including: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| Seismic-related ground failure, including liquefaction - San Gabriel River Bridge Replacement: Potentially significant impact | Mitigation Measure GS-1 would be applicable. GS-1 California Building Code Compliance and Seismic Standards. Prior to grading or building, the Authority, with consultation from MTA Construction staff, shall obtain a soils engineering report(s) prepared by a qualified soils engineer. | With implementation of the Mitigation Measure GS-1, geology & soils impacts would be reduced to a less than significant level. |
| Expose people or structures to potential substantial adverse effect, including the risk of loss, injury, or death involving landslides: No impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| Result in substantial soil erosion or the loss of topsoil: Potentially significant impact (San Gabriel River Bridge and North Colorado Boulevard Bridge Replacements refinements). | Mitigation Measure GS-2 would be applicable. GS-2 Erosion Control. Prior to grading the San Gabriel Bridge Replacement site, erosion control plans should be prepared, with consultation from MTA Construction staff, for any areas where grading on or near significant slopes is planned. See Section 4.10.5 for full text. | With implementation of the Mitigation Measure GS-2, geology & soils impacts would be reduced to a less than significant level. |
| Include structures located on expansive soils, as defined in Section 1802.3.2 of the California Building Code (2007), creating substantial risks to life or property: Potentially significant impact. | Mitigation Measure GS-3 would be applicable. GS-3 Expansive Soils. Prior to grading or building, the applicant shall submit a soils engineering report(s), with consultation from MTA Construction staff, prepared by a qualified soils engineer. See Section 4.10.5 for full text. | With implementation of the Mitigation Measure GS-3, geology & soils impacts would be reduced to a less than significant level. |



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| Environmental Impacts Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in an on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse: No impact Inundation by seiche, tsunami, seismically-induced flooding, or mudflow: No significant impact | Mitigation Measures | Impact Results with Mitigation |
|---|---|---|
| HYDROLOGY & WATER QUALITY | | |
| Violate any Water Quality Standards or Waste Discharge Requirements: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level: Significant impact Substantially Alter the Existing Drainage Pattern of the Site or Area in a Manner Which Would Result in Substantial Erosion or Siltation or Flooding On- or Off-Site: Significant impact Create or Contribute Runoff Water Which Would Exceed the Capacity of Existing or Planned Storm Water Drainage Systems or Provide Substantial Additional Sources of Polluted Runoff: Potentially significant impact | Mitigation Measures WQ-1 through WQ-8 from the 2007 Final EIR as well as WQ-9 would be applicable. WQ-9 As discussed in impact section of 4.8 Utilities, the Authority shall consult with the County, cities, and regional agencies related to stormwater runoff and groundwater and the Urban Water Management Plan to ensure that operation of the proposed Project refinements will not substantially interfere with groundwater recharge or result in a lowering of the groundwater table. | With implementation of the Mitigation Measures WQ-1 through WQ-9 and compliance with federal, state, and other applicable regulatory requirements, hydrology & water quality impacts would be reduced to a less than significant level. |
| Place Housing Within a 100-Year Flood Hazard Area as mapped on a Federal Flood Hazard Boundary or Flood Insurance Rate Map or Other Flood Hazard Delineation Map: No impact Place Structures Within a 100-Year Flood Hazard Area Which Would Impede or Redirect Flood Flows: No impact Expose People or Structures to a Significant Risk of Loss, Injury, or Death Involving Flooding, Including Flooding as a Result of the Failure of a Dam or Levee: Less than significant impact Inundate by Seiche, Tsunami, or Mudflow: No impact | N/A N/A | Impacts would be less than significant. Therefore, no mitigation measures would be required. |
| NOISE & VIBRATION | | |
| Expose persons to or generates noise levels in excess of standards established in a local general plan or noise ordinance or applicable standards of other agencies - M&O Facility Operations, Monrovia and Irwindale parking structure, and the San Gabriel and Colorado River Bridge replacements: Less than significant impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |



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| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|--|--|---|
| <p>Expose persons to or generates noise levels in excess of standards established in a local general plan or noise ordinance or applicable standards of other agencies – M&O Facility: Less than significant</p> <p>Mountain Avenue Realignment Traffic: Significant impact</p> | <p>Mitigation Measures N-1 through N-4 from the 2007 Final EIR as well as N-5 through N-9 would be applicable.</p> <p>N-5 Construction activities within 500 feet of any residences shall be restricted to between the hours of 7:00 AM and 6:00 PM on weekdays and Saturdays with no construction on Sundays and holidays.</p> <p>N-6 All noise-producing project equipment and vehicles using internal combustion engines shall be equipped, where appropriate, with exhaust mufflers and air-inlet silencers in good operating condition that meet or exceed original factory specifications.</p> <p>N-7 Electrically powered equipment shall be used instead of pneumatic or internal combustion powered equipment, where practicable.</p> <p>N-8 Material stockpiles, mobile equipment staging, construction vehicle parking, and maintenance areas shall be located as far as practicable from noise-sensitive land uses.</p> <p>N-9 The erection of temporary noise barriers shall be considered where project activity is unavoidably close to noise sensitive receivers.</p> <p>See Section 4.12.5 for full text.</p> | <p>With implementation of Mitigation Measures N-1 through N-9, construction noise impacts at the M&O Facility in Monrovia would be reduced to a less than significant level. Because of design limitations at Mountain Avenue and Duarte Road, sound walls would not be feasible. As such operational traffic noise impacts would be significant. Therefore, the impact from project-related traffic noise is considered significant and unavoidable.</p> |
| <p>Expose persons to or generate excessive ground-borne vibration or ground borne noise levels: Less than significant</p> | <p>N/A</p> | <p>Impact would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project-</p> <p>M&O Facility: Less than significant</p> | <p>Mitigation Measures N-1 through N-4 from the 2007 Final EIR as well as N-5 through N-9 would be applicable (see above).</p> | <p>See above.</p> |
| <p>Mountain Avenue Realignment Traffic: Significant impact</p> <p>Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project - Monrovia and Irwindale parking structure and the San Gabriel and Colorado River Bridge replacements: Less than significant impact</p> | <p>N/A</p> | <p>Impact would be less than significant. Therefore, no mitigation measures would be required.</p> |
| <p>Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project: Significant impact</p> | <p>Mitigation Measures N-1 through N-4 from the 2007 Final EIR as well as N-5 through N-9 would be applicable (see above).</p> | <p>With implementation of Mitigation Measures N-1 through N-9, construction noise impacts would be reduced to a less than significant level.</p> |



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| Environmental Impacts | Mitigation Measures | Impact Results with Mitigation |
|--|--|--|
| Be located within an airport land use plan area or, where such a plan has not been adopted, within 2 miles of a public airport or public use airport and expose people residing or working in the project area to excessive noise levels; or be located in the vicinity of a private airstrip and expose people residing or working in the project area to excessive noise levels: No impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| RECREATION FACILITIES & PARKS | | |
| Temporary recreation facilities & parks impact: Significant impact (San Gabriel River Trail) | Mitigation Measure R-1 would be applicable. R-1 Temporary closures of the San Gabriel River Trail shall require the development of a detailed detour plan by the design/builder in coordination with the owner/operator of the pathway prior to demolition or construction to minimize impacts to pedestrian and bicycle users of the pathway. The detour plan shall be included in the construction management plan. | With implementation of Mitigation Measure R-1, recreation facilities & parks impacts would be reduced to a less than significant level. |
| Long-term recreation facilities & parks impact: Less than significant | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| BIOLOGY | | |
| Special-Status Plant or Wildlife Species: Potentially significant impact | Mitigation Measure B-6 would be applicable. | With implementation of Mitigation Measure B-6, special-status plant or wildlife species impacts would be reduced to a less than significant level. |
| Sensitive Natural Communities: Significant impact | Mitigation Measures B-6 through B-8 from the 2007 Final EIR would be applicable. | With implementation of Mitigation Measures B-6 through B-8, biology impacts would be reduced to a less than significant level. |
| Wetlands: No impact to wetlands but potential impact to a US ACOE water of the US (San Gabriel River) | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |
| Migratory Birds: Potentially significant impact Local policies or ordinances protecting biological resources: Potentially significant impact | Mitigation Measures B-1 through B-3 from the 2007 Final EIR would be applicable. | With implementation of Mitigation Measures B-1 through B-3, biology impacts would be reduced to a less than significant level. |
| Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan: No impact | N/A | Impact would be less than significant. Therefore, no mitigation measures would be required. |



1.7 Issues to be Resolved

There are several issues to be resolved by the Authority in order to define the parameters of work to be accomplished in the next phase of project development, Preliminary Engineering, and this Final SEIR. These issues are:

- Approval of the six refinements to the Locally Preferred Alternative (LPA) identified in the 2007 Final EIR. This is confirmation that the six refinements best address the Goals and Objectives for the LPA; and
- Approval of the six refinements to be addressed during Preliminary and Final Engineering. This approval can be of the refinements as described in this draft document, or the definition of the refinements can be modified to include variations, a combination of elements, localized options, or other matters raised during the public and agency review and comment process.

1.8 Areas of Controversy

Based on comments received during scoping meeting in 2010, the areas of controversy for the proposed Project refinements focused on potential impacts associated with the M&O Facility and the Mountain Avenue Realignment refinements. The top three issues (along with the typical concerns raised) were:

- Noise: noise walls; noise levels from the M&O Facility; concerned about construction noise
- Displacements: homes to be displaced at Mountain Avenue Realignment; right-of-way acquisition/relocation process
- Aesthetics/Compatible Land Uses: concerns about the appearance of the M&O Facility and compatibility with adjacent land uses; suggested landscape treatments for all applicable refinements.

Other comments addressed in a general, non-specific manner: relocation; safety; M&O Facility design and location; property values; community impacts; operational hours; and noise from road crossings.

1.9 Environmentally Superior Alternative

Section 15126.6 (e)(2) of the State CEQA Guidelines requires that an environmentally superior alternative be identified among the selected alternatives (excluding the No Build alternative). The Environmentally Superior Alternative as discussed in this SEIR is the implementation of the proposed Project Phase 2A refinements, as described in Chapter 3 Project Description, which includes construction of the M&O Facility in Monrovia and five additional project refinements. The objectives of the proposed Project include the development of an M&O Facility to accommodate LRT system capacity and storage requirements and perform routine and special maintenance as well as light and heavy duty repairs for LRVs. It also defines the realignment of the Mountain



Avenue/Duarte Road intersection for safety purposes; the relocation of parking facilities for the Monrovia and Irwindale stations; and the replacement of the Colorado Boulevard and San Gabriel River bridges.

~~A comparison of the impacts associated with the proposed Project M&O Facility refinement in Monrovia and the M&O Facility in Irwindale (Alternative 2) is described in the table below.~~

Impacts to sensitive biological resources, including bird species protected under the MBTA would be greater in Alternative 2. In addition, impacts relative to hydrology and water quality, specifically the potential for flooding, are also greater with Alternative 2. Lastly, due to the former use of the Irwindale site as a quarry, the stability of slopes and soils within the site poses a significant risk to worker safety during construction and operation of the M&O Facility in Irwindale, which may result in the need to incorporate slope stabilizing measures throughout large portions of the site.

Overall, development of Alternative 2 on the largely undeveloped M&O Facility site in Irwindale (Alternative 2) has the potential to result in greater environmental impacts relative to biological resources, hydrology and water quality, and geology and soils. Given that both sites meet the project objectives, the comparison of the two sites is largely dependent on the environmental impacts associated with construction and operation of the M&O Facility at either the Monrovia site (proposed Project refinement) or the Irwindale site (Alternative 2). For the reasons stated above, the proposed M&O Facility in Monrovia is the environmentally superior alternative.

1.9.1 M&O Facility

The objectives of the proposed Project includes the development of an M&O Facility to accommodate the LRT system capacity and storage requirements, to perform routine and special maintenance, and to accomplish light and heavy duty repairs for LRVs.

1.9.2 Other Project Refinements

In addition to the M&O Facility the proposed Project includes refinements to the Mountain Avenue/Duarte Road intersection for safety purposes; relocation of parking facilities for the Monrovia LRT Station Parking Structure and Irwindale LRT Station Parking Structure or Surface Lot; San Gabriel River Bridge Replacement; and the North Colorado Blvd. Bridge Replacement.

Other refinements:

- Realign the Mountain Ave./Duarte Rd. intersection to improve safety,
- Relocate parking at Monrovia Station to better accommodate the City of Monrovia's future transit oriented development (TOD),
- Relocate parking location and configuration at Irwindale station and improve safety and constructability at the Irwindale Station,
- Replace the Colorado Boulevard Bridge to address structural issues and minimize property requirements, and
- Replace the San Gabriel River Bridge design and structural issues.



1.10 Permits and Approvals

Table 1-2: Permitting Requirements

| Agency | Type of Permit/Authority |
|---|--|
| U.S. Army Corps of Engineers | Clean Water Act, Section 404 |
| Regional Water Quality Control Board | Clean Water Act, Section 401 Porter Cologne Water Quality Control Act |
| Regional Water Quality Control Board | Clean Water Act, Section 402 Porter Cologne Water Quality Control Act |
| <u>Regional Water Quality Control Board</u> | <u>Clean Water Act, Section 404</u> <u>Porter Cologne Water Quality Control Act</u> |
| Regional Water Quality Control Board | National Pollutant Discharge Elimination System Permits |
| Office of Historic Preservation | National Historic Preservation Act, Section 106 |
| California Department of Fish and Game | Fish and Game Code, Section 1602 California Endangered Species Act, Section 2081 |





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Fort Worth, Texas 76131-2828
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Email - john.sims@bnsf.com

April 29, 2011

City of Arcadia
Planning Commission
240 West Huntington Dr.
P.O. Box 60021
Arcadia, CA 91066

**Re: STB Docket No. AB-6 (Sub-No. 477X) BNSF Railway Company –
Abandonment Exemption – in Los Angeles County, California**

BNSF Railway Company ("BNSF") anticipates filing in the near future a Notice of Exemption seeking Surface Transportation Board ("STB") authority in the above-referenced docket to abandon 4.85 miles of rail line in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale.

As part of the environmental report, BNSF is required to contact you to determine if the proposed abandonment is consistent with existing land use plans. If applicable, please describe any inconsistencies.

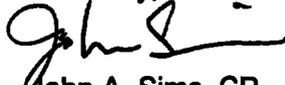
The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, a CD with an electronic copy of the Environmental Evaluation, and a map of the subject railroad line. The entire reports can be viewed at the following web link:

http://www.foothillextension.org/construction_phases/phase_2a_pasadena_to_azusa/supplemental-environmental-impact-report/2010-draft-supplemental-environmental-impact-report/

Please provide your assessment and comments to me at the address above, if at all possible, by May 13, 2011. You may contact me by email or phone with any questions or concerns.

Thank you in advance for your time and contribution.

Sincerely,


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Paralegal

Enclosures as stated

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April 29, 2011

City of Irwindale
Planning Commission
5050 North Irwindale Avenue
Irwindale, CA 91706

**Re: STB Docket No. AB-6 (Sub-No. 477X) BNSF Railway Company –
Abandonment Exemption – in Los Angeles County, California**

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April 29, 2011

Richard J. Bruckner, Director
Department of Regional Planning
County of Los Angeles
1390 Hall of Records
320 West Temple Street
Los Angeles, CA 90012

**Re: STB Docket No. AB-6 (Sub-No. 477X) BNSF Railway Company –
Abandonment Exemption – in Los Angeles County, California**

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The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, a CD with an electronic copy of the Environmental Evaluation, and a map of the subject railroad line. The entire reports can be viewed at the following web link:

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Email john.sims@bnsf.com

April 29, 2011

Lincoln E. Burton, State Conservationist
California NRCS State Office
430 G Street #4164
Davis, CA 95616-4164

**Re: STB Docket No. AB-6 (Sub-No. 477X) BNSF Railway Company –
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As part of the requisite environmental report, BNSF needs to know whether or not the proposed abandonment will have any effect on prime agricultural lands. Your assessment and comments are respectfully requested.

The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, a CD with an electronic copy of the Environmental Evaluation, and a map of the subject railroad line. The entire reports can be viewed at the following web link:

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Email - john.sims@bnsf.com

April 29, 2011

U.S. Fish & Wildlife Service
Sacramento Fish & Wildlife Office
2800 Cottage Way, Room W-2605
Sacramento, CA 95825

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As part of the requisite environmental report, BNSF needs to know: 1) whether or not there are any endangered or threatened species, wildlife sanctuaries or refuges, or areas designated as critical habitat adjacent to or near the line, and 2) if so, what effects the proposed action may have on same.

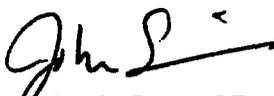
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April 29, 2011

U.S. Department of the Interior
Bureau of Land Management
California State Office
2800 Cottage Way, Suite W-1623
Sacramento, CA 95825-1886

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April 29, 2011

Ms. Christine Lehnertz, Regional Director
U.S. Department of the Interior
National Park Service
One Jackson Center
1111 Jackson Street, Suite 700
Oakland, CA 94607

**Re: STB Docket No. AB-6 (Sub-No. 477X) BNSF Railway Company –
Abandonment Exemption – in Los Angeles County, California**

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As part of the requisite environmental report, BNSF needs to know: 1) whether or not there are any wildlife sanctuaries or National or State parks or forests adjacent to or near the line, and 2) if so, what effects the proposed action may have on same.

The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, a CD with an electronic copy of the Environmental Evaluation, and a map of the subject railroad line. The entire reports can be viewed at the following web link:

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April 29, 2011

U.S. EPA Region 9
75 Hawthorne Street
San Francisco, CA 94105

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As part of the requisite environmental report, BNSF needs to know: 1) whether or not this action will be consistent with Federal, State or local water quality standards, and 2) whether or not Section 402 and/or National Pollutant Discharge Elimination System ("NPDES") permits are required for performance of the salvage activity described below.

The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, a CD with an electronic copy of the Environmental Evaluation, and a map of the subject railroad line. The entire reports can be viewed at the following web link:
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April 29, 2011

California Department of Water Resources
1416 9th Street
Sacramento, CA 95814

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As part of the environmental report, BNSF is required to contact your agency to determine if the proposed abandonment:

1. will affect land or water uses within a designated coastal zone;
2. is consistent with applicable Federal, State or local water quality standards (with a description of any inconsistencies identified); and
3. will require permits under Section 402 of the Clean Water Act (33 U.S.C. § 1342).

The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, a CD with an electronic copy of the Environmental Evaluation, and a map of the subject railroad line. The entire reports can be viewed at the following web link:

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Email – john.sims@bnsf.com

April 29, 2011

U.S. Army Corps of Engineers
Los Angeles District
915 Wilshire Blvd., Suite 1101
Los Angeles, CA 90017

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As part of the requisite environmental report, BNSF needs to know: 1) whether or not Section 404 permits will be required for the performance of salvage activity, and 2) if the proposed abandonment will affect any 100-year floodplains or any designated wetlands. Your assessment and comments are respectfully requested. In addition, if it is your determination that floodplains will be affected please furnish, if available, 8½" x 11" black and white maps of each designated floodplain area. Please note: **BNSF does not anticipate any potential impacts to waters of the U.S. as a result of the proposed abandonment.**

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**SUPPLEMENTAL HISTORIC PROPERTIES
SURVEY AND EFFECTS REPORT**

for the

**Gold Line Foothill Extension Project
(Pasadena to Montclair)
Formerly Gold Line Phase II Project
Los Angeles and San Bernardino Counties, California**



PREPARED FOR

Federal Transit Administration (FTA)

and

**Los Angeles to Pasadena Blue Line Construction Authority
Metro Gold Line Foothill Extension Construction Authority**

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Gold Line Foothill Extension – Pasadena to Montclair

SUPPLEMENTAL HISTORIC PROPERTIES SURVEY AND EFFECTS REPORT FOR THE GOLD LINE FOOTHILL EXTENSION - PASADENA TO MONTCLAIR PROJECT Los Angeles and San Bernardino Counties, California

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Introduction

The purpose of this Supplemental Historic Property Survey and Effects Report (SHR) is to provide supplemental and revised information about historic properties related to recent design changes to the Gold Line Foothill Extension Project – Pasadena to Montclair. The Federal Transit Administration (FTA) and the California State Historic Preservation Officer (SHPO) previously made a consensus finding that the undertaking would result in “no adverse effect” on historic properties. (See letter dated July 1, 2004, in Appendix A.) FTA believes that the 2005 design changes to the undertaking will not change the previous finding of “no adverse effect”, and this SHR has been prepared to request the concurrence of SHPO pursuant to Section 106 of the National Historic Preservation Act (NHPA) and its implementing guidelines (36 CFR Part 800, as amended through August 5, 2004).

For convenience, the 2005 design changes are illustrated and noted on each Area of Potential Effects (APE) Map. (See Appendix B.)

Project Description

The undertaking proposed by the Los Angeles to Pasadena Metro Blue Line Construction Authority (Authority) is an approximately 24-mile long light rail transit extension from Pasadena to Montclair. The proposed corridor generally follows the foothills of the San Gabriel Mountains in Los Angeles County, beginning from the Metro Gold Line station in Sierra Madre Villa in Pasadena, to approximately one mile east of the San Bernardino County line to the Metrolink Station and transit Center at Central Avenue in Montclair. The proposed project is located primarily along the existing Burlington Northern Santa Fe (BNSF) railroad right-of-way, paralleling Interstate 210 and Arrow Highway.

A general Study Area was defined to encompass 13 adjoining cities that lie along I-210 and a railroad right-of-way, between Pasadena on the west and Montclair/Upland on the east. The study area includes the cities of Pasadena, Arcadia, Monrovia, Duarte, Irwindale, Azusa, Glendora, San Dimas, La Verne, Pomona and Claremont in Los Angeles County. In San Bernardino, it includes the cities of Montclair and Upland.

The Full Build Alternative encompasses Segments 1 and 2 and extends the current Gold Line system from Sierra Madre Villa Station in Pasadena to the Montclair TransCenter (approximately 24 miles). Segment 1 lies between the current Sierra Madre Villa Station and the proposed Azusa Station and is about 11.4 miles in length. Segment 2 lies between the Azusa Station and the existing Montclair TransCenter. The same LRT technology and the same types of system components would be used as are in the existing Phase I segment from Los Angeles to Pasadena, and in the soon-to-be built Eastside Extension. The Eastside Extension will run from Union Station to Beverly/Atlantic in East Los Angeles.

Approximately sixteen (16) traction power substations (TPSSs) would be constructed along the route in order to provide electrical power to the line. Where possible, TPSS sites would be located near a station. TPSS sites would be located within the existing rail ROW or within properties to be acquired for stations or parking.

Regulatory Framework

a. Section 106 of the National Historic Preservation Act

The National Environmental Policy Act (NEPA) requires that federal agencies integrate the NEPA process with other environmental laws. Section 106 of the National Historic Preservation Act as amended (Section 106, 16 U.S.C. 470f) requires that impacts on historic properties be taken into consideration in any federal undertaking. "Historic property means any prehistoric or historic district, site, building, structure, or object included in, or eligible for inclusion in, the National Register of Historic Places (National Register) maintained by the Secretary of the Interior. This term includes artifacts, records, and remains that are related to and located within such properties. The term includes properties of traditional religious and cultural importance to an Indian tribe or Native Hawaiian organization that meet the National Register criteria" [36 CFR §800.16(l)].

Cultural resources studies for the proposed Metro Gold Line Foothill Extension Project - Pasadena to Montclair are subject to the procedures of and review of the Federal Transit Administration (FTA) in consultation with the California State Historic Preservation Officer (SHPO). These studies are shaped by the Advisory Council on Historic Preservation (ACHP) regulations (36 CFR Part 800, as amended through August 5, 2004) for implementing Section 106. Section 106 studies provide the information necessary to satisfy legal requirements for environmental documents under NEPA.

Previous Section 106 correspondence is located in Appendix A.

Compliance Methodology

a. The Area of Potential Effects

As defined in the Section 106 regulations, the Area of Potential Effects (APE) means "the geographic area or areas within which an undertaking may directly or indirectly cause changes in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects cause by the undertaking" [36 CFR §800.16(d)].

On September 16, 2003, FTA sent a letter to the SHPO to determine and document the preliminary APE on an aerial base map. In a letter dated November 5, 2003, (see Appendix A) SHPO concurred with the APE definition for the proposed project as follows:

"The Federal Transit Administration (FTA) has identified seventeen (17) updated (2005) Areas of Potential Effects (APEs) that are located along the proposed project corridor. These project APEs would be delineated to:

- Include all parcels directly affected by or adjacent to proposed station areas, construction staging areas, and acquisition areas that are not part of the existing railroad right-of-way.
- Include all bridges that require alteration other than track work for the proposed project.
- Those areas of ground that would be disturbed during project construction, excluding railroad tracks, ballast, ties, and equipment less than 50 years of age."

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The APE Maps revised with 2005 design change are included in Appendix B. The 2005 changes to the APE boundaries are shown on each APE Map as a highlighted transparent yellow section with a double yellow line. For convenience, please refer to the box inserts labeled “2005 APE Note” on each APE Map to better understand design changes that have occurred.

b. Identify Consulting and Interested Parties

The Section 106 regulations require that a federal agency evaluate all properties within the APE and identify historic properties by gathering information from consulting parties, applying the National Register Criteria, and seeking concurrence from the SHPO or Indian tribe, as appropriate.

Specifically for Section 106, letters were sent to other potentially interested parties listed below on November 7, 2003, and again on May 23 & 31, 2005, to identify the 2005 design changes.

- AIA Los Angeles
- Arcadia Historical Society
- Azusa Historical Society
- California Historical Society
- California Preservation Foundation
- California State Railroad Museum
- Chinese Historical Society
- Claremont Heritage, Inc.
- City of Arcadia Development Services Department
- City of Azusa Community Development Department
- City of Claremont Planning Department
- City of Duarte Community Development Department
- City of Glendora Planning Department
- City of Irwindale Planning Department
- City of La Verne
- City of Los Angeles Community Redevelopment Agency
- City of Los Angeles Cultural Heritage Commission
- City of Los Angeles Planning Department
- City of Monrovia Community Development Department
- City of Montclair Community Development Department
- City of Pomona Planning Department
- City of San Dimas
- Cooper Museum/Chaffey Communities Cultural Center
- Duarte Historical Society, Museum & Friends of the Duarte Library
- Glendora Community Conservancy
- Glendora Historical Society
- Historical Society of Pomona Valley
- Historical Society of Southern California

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- La Verne Heritage Foundation
- Lomita Railroad Museum
- Los Angeles City Historical Society
- Los Angeles Conservancy
- Los Angeles County Historic Landmarks and Records Commission
- Los Angeles Forum for Architecture and Urban Design
- Los Angeles Railroad Heritage Foundation
- Monrovia Historical Society
- Monrovia Old House Preservation Group
- Pacific Railroad Society
- Pasadena Heritage
- Pomona Heritage
- Rivers and Mountains Conservancy
- San Bernardino Railroad Historical Society
- San Dimas Historical Society
- San Dimas Pacific Railroad Museum
- Sierra Club, Los Angeles Chapter
- Sierra Madre Historical Society
- Society of Architectural Historians, Southern California Chapter
- Southern Pacific Historical & Technical Society
- Train Riders Association of Southern California
- Train Web, Inc.
- The Transit Coalition
- The Transportation and Land Use Collaborative of Southern California
- Travel Town Transportation Museum
- Wheel Clicks.

Response letters, for 2005, were received from the Cities of Arcadia, Glendora, Duarte, Irwindale, Claremont, Montclair, and from the San Gabriel Rivers and Mountains Conservancy. These letters are summarized below:

The City of Arcadia, from a return fax document dated June 3, 2005, states “We do not have any historical records of buildings or structures in the APE designated on your aerials. However, the buildings along the north side of Huntington Drive between Santa Anita Avenue and First Avenue are over 50 Years old and have some historical significance to the City.”

The City of Glendora, from a letter dated July 5, 2005, states “There are no historic properties within the boundaries identified in the exhibit.”

The City of Duarte, from a return fax document dated June 1, 2005, states “No significant change from previous map. No apparent resources affected.”

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The City of Irwindale, from a return fax document dated June 6, 2005, states “No comments.”

The City of Claremont, from a return fax document dated July 6, 2005, states “The Sumner House at 105 N. College Avenue is just beyond the yellow boundary.” Note: This historical resource was within the previous 2004 APE boundary and has been included within the 2005 Claremont APE Map.

The City of Montclair, from a return fax document dated May 31, 2005, states “No comments regarding historical resources.”

The Rivers and Mountains Conservancy, from a letter dated on June 6, 2005, states “I have reviewed the maps you conveyed and have not identified any areas of specific concerns based on the modifications that have been made to the current alignment. However, it is important to note that the RMC would encourage and support an alignment that does not impact historical and cultural resources including but not limited to structure, parks and open space.”

As of August 2005, no other written responses were received from the parties listed above.

c. National Register Criteria for Evaluation

In order for a property to be considered for inclusion in the National Register it must meet the criteria for evaluation set forth in 36 CFR Part 60.4, as follows:

The quality of significance in American history, architecture, archaeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of design, setting, materials, workmanship, feeling, and association and

(a) that are associated with events that have made a significant contribution to the broad patterns of our history; or

(b) that are associated with the lives of persons significant in our past; or

(c) that embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or

(d) that have yielded, or may be likely to yield, information important in prehistory or history.

Among other criteria considerations, a property which has achieved significance within the last 50 years is not considered eligible for inclusion in the National Register unless certain exceptional conditions are met. The 50-year age criterion for the proposed project has been set at 1954.

d. Identifying Historic Properties

For the proposed project, surveys have been undertaken and documentation prepared in accordance with the Secretary of Interior's Standards and Guidelines for Identification of Historic Properties (48 FR 44716), using personnel who meet the Secretary of Interior's Professional Standards (48 FR 22716) in the fields of ethnography, pre-historic archaeology, historic archaeology, architectural history, and history.

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For the purposes of this document, the broad pool of cultural resources within the APE that require evaluation for National Register eligibility may be categorized into two major types, as follows:

- 1) Archaeological Resources, which include resources that represent important evidence of past human behavior, including portable artifacts such as arrowheads or tin cans; non-portable “features” such as cooking hearths, foundations, and privies; or residues such as food remains and charcoal. Archaeological remains can be virtually any age, from yesterday’s trash to prehistoric deposits thousands of years old.
- 2) Historic and Architectural Resources, which include man-made features that comprise the recognizable built environment. This category typically includes extant, aboveground buildings and structures that date from the earliest territorial settlements until the present day.

Archaeological Resources

The 2005 design changes did not require revisions to previous archaeological analyses and findings.

Historic and Architectural Resources

a. Identification Methodology

Records Search

A background research survey was undertaken to identify previously documented historic and architectural resources within and near the APE and to help establish a context for resource significance. National, state and local inventories of architectural/historic resources were examined in order to identify significant local historical events and personages, development patterns, and unique interpretations of architectural styles. The following inventories and sources were consulted:

- The National Register of Historic Places, National Register Information System
- California Register of Historical Resources
- California Office of Historic Preservation Historical Resources Inventory System
- California Historical Landmarks
- California Points of Historical Interest
- City of Claremont Historic-Cultural Monuments
- City of La Verne Planning Department files
- City of Azusa Planning Department files
- City of Pomona Planning Department files.
- City of Glendora list of Designated Historic Landmarks
- City of San Dimas Planning Department files

Field Survey

A field survey of all properties within the updated (2005) APE was undertaken according to standard Section 106 regulations and related procedures. Qualified architectural historians conducted field investigations on multiple occasions in 2005. David Greenwood conducted field investigations and building permit research for the updated (2005) APE Boundary Area, and the Azusa and Pomona Flyover

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locations. During the field investigations, the boundaries of the APE were confirmed, and an assessment was made of all extant buildings and structures within the updated APE to determine if their age and integrity warranted application of National Register criteria.

The field survey of historic and architectural resources included the following steps:

- A field survey consisting of a visual onsite examination of every parcel within the APE, including an assessment of integrity.
- Identification of the age of all major buildings, structures, objects, and potentially coherent districts located within the APE.
- Photography of each potential district feature, major structure, building, or object within the APE.
- Review in the field of previous survey data, comments from interested parties, and lists of significant historic properties.
- Following the field survey, site-specific research was conducted from the following sources:
 - City Directories of Los Angeles County, California.

In addition, information was requested from John Signor, Railroad Historian.

b. Significant Historic and Architectural Resources Identified

The results of the records search, background research and field survey by qualified architectural historians was recorded on California Historic Resource Inventory forms (Series DPR 523), which are included as an Appendix F to this technical report. The records search, field surveys, and subsequent research resulted in the following findings.

- Forty (40) properties with buildings or structures constructed in or before 1954 that do not meet National Register criteria because either they do not retain integrity from their period of significance, or are not associated with an important historic context.
- The remaining properties in the APE are improved with buildings constructed in or after 1955. Such properties are not eligible for the National Register because they possess no known association with an important historic context that would override the National Register's 50-year age criterion consideration.

Properties listed in the National Register or determined eligible for listing in the National Register are automatically listed in the California Register. The final determination of historic properties listed below is subject to change as a result of Section 106 consultation with the SHPO regarding National Register eligibility, which is pending concurrence with FTA's findings in this document.

FTA requests your concurrence with the findings presented in Appendix F and in the following table, which identifies all properties in the updated (2005) APE, that do not meet National Register criteria. These properties do not retain integrity from their period of significance, nor exhibit unique architectural quality, distinction, or historic character, or are not associated with an important historic context.

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| PROPERTIES IN THE REVISED (2005) APE FOUND NOT TO MEET CRITERIA FOR LISTING IN THE NATIONAL REGISTER OF HISTORIC PLACES | | |
|--|---|---|
| Address APE Map Figure and APN | Resource Name and Year Built | California Historical Resource Status Code, pending SHPO concurrence |
| PASADENA | | |
| Plate Girder Span at Rosemead Blvd, Arcadia. (NOTE: There is no APE MAP for Pasadena) | Name: AT & SF Railroad bridge over Rosemead Boulevard Year Built: 1950 – 1957 Estimated. | 6Y |
| ARCADIA STATION | | |
| 33 St. Joseph Street, Arcadia, Ca. APE Map Figure 3-5.2 Parcel No. 5773-005-025. | Name: J & R Engineering Year Built: 1953 | 6Y |
| 25-31 Flower Street, Arcadia, Ca. APE Map Figure 3-5.2 Parcel No. 5773-005-024. | Name: Pratt Construction Company Year Built: 1953 | 6Y |
| Colorado Boulevard bridge, Arcadia, Ca. APE Map Figure 3-5.1 | Name: AT&SF Railway bridge over Colorado Boulevard Year Built: 1933 | 5S2 |
| Huntington Drive Bridge, Arcadia, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT&SF Railroad bridge over Huntington Drive Year Built: 1942 | 6Y |
| MONROVIA STATION | | |
| Santa Anita Wash Bridge, Monrovia, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT&SF Railroad bridge over Santa Anita Wash Year Built: 1915 | 6Y |

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| PROPERTIES IN THE REVISED (2005) APE FOUND NOT TO MEET CRITERIA FOR LISTING IN THE NATIONAL REGISTER OF HISTORIC PLACES | | | |
|--|---|---|--|
| Address APE Map Figure and APN | Resource Name and Year Built | California Historical Resource Status Code, pending SHPO concurrence | |
| Sawpit Wash Bridge, Monrovia, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT&SF Railroad bridge over Sawpit Wash Year Built: 1941 | 6Y | |
| 5 th Avenue Pedestrian Underpass, Monrovia, Ca. APE Map Figure 3-5.8 | Name: 5 th Avenue Pedestrian Tunnel under AT&SF Railroad Year Built: 1942 | 5S2 | |
| Alta Vista Wash Deck Beam Bridge, Monrovia, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT&SF Railroad bridge over Alta Vista Wash Year Built: 1907 | 6Y | |
| DUARTE STATION | | | |
| 1559 Three Ranch Road, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-009-036. | Name: Home for Anthony L. Sposato Year Built: 1949 | 6Y | |
| 1614 Glenford Avenue, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-010-063. | Name: 1614 Glenford Avenue Year Built: 1948 | 6Y | |
| 1615 Glenford Avenue, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-010-062. | Name: 1615 Glenford Avenue Year Built: 1948 | 6Y | |
| 1616 Fairdale Avenue, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-010-055. | Name: 1616 Fairdale Avenue Year Built: 1948 | 6Y | |
| 1628 Fairdale Avenue, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-009-026 | Name: 1628 Fairdale Avenue Year Built: 1949 | 6Y | |

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| PROPERTIES IN THE REVISED (2005) APE FOUND NOT TO MEET CRITERIA FOR LISTING IN THE NATIONAL REGISTER OF HISTORIC PLACES | | |
|--|---|---|
| Address APE Map Figure and APN | Resource Name and Year Built | California Historical Resource Status Code, pending SHPO concurrence |
| 1636 Fairdale Avenue, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-009-027. | Name: 1636 Fairdale Avenue Year Built: 1949 | 6Y |
| 1640 Fairdale Avenue, Duarte, Ca. APE Map Figure 3-5.5 Parcel No. 8528-009-028 | Name: 1640 Fairdale Avenue Year Built: 1949 | 6Y |
| IRWINDALE STATION | | |
| San Gabriel River bridge, Irwindale, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT&SF Railroad bridge over San Gabriel River in Irwindale Year Built: 1903 | 6Y |
| AZUSA STATION | | |
| 627 N. Vernon Avenue, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-041 | Name: Residence for Charles R. Smith Year Built: 1927 | 6Y |
| Foothill Boulevard bridge, Azusa, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT&SF Railroad bridge over Foothill Blvd Year Built: 1942 | 6Y |
| 630 N. Vernon Avenue, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-025 | Name: 630 N. Vernon Avenue Year Built: 1953 | 6Y |
| 623 N. Vernon Avenue, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-034 | Name: Residence for Herbert S. Zadech Year Built: 1927 | 6Y |

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| PROPERTIES IN THE REVISED (2005) APE FOUND NOT TO MEET CRITERIA FOR LISTING IN THE NATIONAL REGISTER OF HISTORIC PLACES | | | California Historical Resource Status Code, pending SHPO concurrence |
|--|---|----|---|
| Address APE Map Figure and APN | Resource Name and Year Built | | |
| 750 W. Foothill Blvd., Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-004 | Name: BTW Performance Year Built: 1929 | 6Y | |
| 819 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-013 | Name: Speculative Duplex for Jim M.H. & Bessie M. Druggiero Year Built: 1947 | 6Y | |
| 826 N. Azusa Avenue, Azusa, Ca. APE Map Figure 3-5.9 Parcel No. 8616-003-014 | Name: 826 N. Azusa Avenue Year Built: 1952 | 6Y | |
| 832 N. Azusa Avenue, Azusa, Ca. APE Map Figure 3-5.9 Parcel No. 8616-003-011 | Name: Last Chance Saloon Year Built: 1954 | 6Y | |
| 833 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-012 | Name: Speculative Duplex for Jim M.H. & Bessie M. Druggiero Year Built: 1947 | 6Y | |
| 845 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-011 | Name: Speculative Duplex for Jim M.H. & Bessie M. Druggiero Year Built: 1947 | 6Y | |
| 855 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-010 | Name: Speculative Duplex for Frank D. Richards Year Built: 1947 | 6Y | |
| 865 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-009 | Name: Speculative Duplex for Frank D. Richards Year Built: 1947 | 6Y | |

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| PROPERTIES IN THE REVISED (2005) APE FOUND NOT TO MEET CRITERIA FOR LISTING IN THE NATIONAL REGISTER OF HISTORIC PLACES | | | |
|--|---|---|--|
| Address APE Map Figure and APN | Resource Name and Year Built | California Historical Resource Status Code, pending SHPO concurrence | |
| 877-879 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-008 | Name: Speculative Duplex for Frank D. Richards Year Built: 1947 | 6Y | |
| 885-887 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-007 | Name: Speculative Duplex for Frank D. Richards Year Built: 1947 | 6Y | |
| 893-895 W. 6 th Street, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-006 | Name: Speculative Duplex for Frank D. Richards Year Built: 1947 | 6Y | |
| 900 W. Foothill Blvd, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-006 | Name: Quality Material Handling Inc. Year Built: 1931 | 6Y | |
| 972 W. Foothill Blvd, Azusa, Ca. APE Map Figure 3-5.8 Parcel No. 8616-003-006 | Name: Ashton Door & Glass Year Built: 1948 | 6Y | |
| GLENDORA STATION | | | |
| San Dimas Wash Bridge, Glendora, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT & SF Railroad bridge over San Dimas Wash Year Built: 1914 | 6Y | |
| Foothill Boulevard Bridge, Glendora, Ca. (NOTE: There is no APE MAP for this bridge). | Name: AT & SF Railroad bridge over Foothill Boulevard Year Built: 1929 | 6Y | |

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| PROPERTIES IN THE REVISED (2005) APE FOUND NOT TO MEET CRITERIA FOR LISTING IN THE NATIONAL REGISTER OF HISTORIC PLACES | | |
|--|---|---|
| Address APE Map Figure and APN | Resource Name and Year Built | California Historical Resource Status Code, pending SHPO concurrence |
| POMONA STATION | | |
| 2692 N. Towne Avenue, Pomona, Ca. APE Map Figure 3-5.15 Parcel No. 8313-001-008 | Name: Ace Hardware (C&E) Year Built: 1954 | 6Y |
| 120 E. Santa Fe Street, Pomona, Ca. APE Map Figure 3-5.14 Location: Loop and Meserve Tract Lot 1. | Name: Santa Fe Pomona Station Year Built: 1940 | 3CS |
| CLAREMONT STATION | | |
| 111 College Avenue, Claremont, Ca. APE Map Figure 3-5.16 Parcel No. 8313-022-001 | Name: Courier/Claremont Forum Year Built: 1947 | 6Y |
| MONTCLAIR STATION | | |
| 4974 Arrow Highway, Claremont, Ca. APE Map Figure 3-5.17 Parcel No. 1007-701-02-0000 | Name: Thompson Plumbing Supply Year Built: 1958 | 6Y |
| 5040-5050 Arrow Highway, Claremont, Ca. APE Map Figure 3-5.17 Parcel No. 1007-701-02-0000 | Name: Inland Pacific Ballet Year Built: 1955 | 6Y |

Description of Construction Activities

Phase I

The cities in Phase I are Los Angeles, South Pasadena and Pasadena. There are no physical elements of the Build LRT to Azusa Alternative or the Full Build (Pasadena to Montclair) Alternative. No new construction would take place within the extent of the Phase I portion of the Project, that is the already-existing Gold Line.

Foothill Extension, Segment 1

The cities in the Foothill Extension, Segment 1 are Pasadena, Arcadia, Monrovia, Duarte, Irwindale, and Azusa. LRT stations in Segment 1 would include the existing station at Sierra Madre Villa in Pasadena, and new stations in Arcadia, Monrovia, Duarte, Irwindale, and Azusa.

Foothill Extension, Segment 2

The cities in the Foothill Extension, Segment 2 are Glendora, San Dimas, La Verne, Pomona, Claremont, Montclair, and Upland. The Full Build Alternative would include LRT stations in each community, except for a joint station serving Montclair and Upland.

Criteria of Adverse Effect

In order to comply with Section 106 of the National Historic Preservation Act, any effects of the proposed undertaking on properties listed in or determined eligible for inclusion in the National Register must be analyzed by applying the Criteria of Adverse Effect [36 CFR Part 800.5(a)], as follows:

(1) Criteria of adverse effect. An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property's eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance or be cumulative.

(2) Examples of adverse effects. Adverse effects on historic properties include, but are not limited to:

(i) Physical destruction of or damage to all or part of the property;

(ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines;

(iii) Removal of the property from its historic location;

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(iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;

(v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features;

(vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and

(vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

The above criteria apply to archaeological, historic and architectural resources.

Application of the Criteria of Adverse Effect

These examples of the Criteria of Adverse Effect are described below as they pertain to the proposed **Build LRT to Azusa Alternative** and **Full Build Alternative**.

(i) Physical destruction of or damage to all or part of the property;

No known historic properties would be demolished or damaged as a result of the proposed project, therefore Section 106 *criteria example (i)* would not apply.

However, cultural resources are known to exist within or adjacent to the Foothill Extension Project APE, and there is a potential for unknown subsurface structural remains or prehistoric sites within the Project APE. Grading for parking lots or construction may expose buried, unrecorded cultural resources. The physical removal and destruction of significant structural remains, artifacts and features, if found in settings retaining integrity, would result in an adverse effect finding under Section 106 if they are found to meet National Register Criteria. Thus, there is a potential to destroy or damage unknown cultural resources during construction on Phase II, Segment 1.

(ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines;

No historic property would be altered as a result of the proposed project, therefore Section 106 *criteria example (ii)* would not apply.

(iii) Removal of the property from its historic location;

No historic property would be removed from its historic location for the proposed project, therefore Section 106 *criteria example (iii)* would not apply.

(iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;

The proposed project would be constructed within an existing railroad right of way and would continue rail operations there. The proposed station platforms and associated

Gold Line Foothill Extension – Pasadena to Montclair

waiting canopies, benches, ticket kiosks and centenary wire support poles would be compatible with the historic character of the railroad ROW and proposed station areas. Proposed parking lots and structures would be constructed on existing industrial or vacant parcels and would be sufficiently distanced from historic properties. Therefore, there would be no change of the character of any historic property's use. The proposed project would not change any contributing physical features within any of the historic property's settings that contribute to their significance. Therefore Section 106 *criteria example (iv)* would not apply.

(v) Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;

Under Section 106, only Criteria of Adverse Effect example (v), Introduction of Visual, Atmospheric, or Audible Elements, warrants further discussion with regard to the application of the Criteria for Adverse Effect to the historic properties within the APE for the proposed project. Atmospheric and audible elements would continue to be generated by train traffic, and vehicular traffic near all the proposed station areas, and historic properties, with no demonstrable change from current conditions; therefore no further discussion is necessary.

Below is a discussion of the potential project related visual effects under criteria example (v), on each of the historic properties located within the APE of the proposed project.

(vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and

No historic property would be neglected by the proposed project, therefore Section 106 *criteria example (vi)* would not apply.

(vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

None of the historic properties within the APE are Federally owned or controlled, therefore Section 106 *criteria example (vii)* would not apply.

The 2005 design changes are explained below, with an application of the Criteria for Adverse Effect.

Gold Line Foothill Extension – Pasadena to Montclair

Stuart Company Plant and Office Building (Johnson & Johnson/Merck Consumer Pharmaceutical Building), 3360 E. Foothill Blvd., Pasadena

(Unchanged in 2005)

The proposed project's double track would be located within the existing railroad ROW between the east and westbound lanes of the 210 Freeway, approximately 400 feet to the south of the southern elevation of the Stuart Company Plant Office Building. The existing 5-level parking structure for the Sierra Madre Villa station is located immediately north of the 210 Freeway, between the proposed double track alignment and the Stuart Company Plant Office Building. Because of this relatively great distance, and because the existing parking structure blocks views to and from the Stuart Company Plant Office Building, there would be no visual change to this historic property or its setting. Therefore, there would be no effect under Section 106 *criteria example (v)*. (see Figure 1)

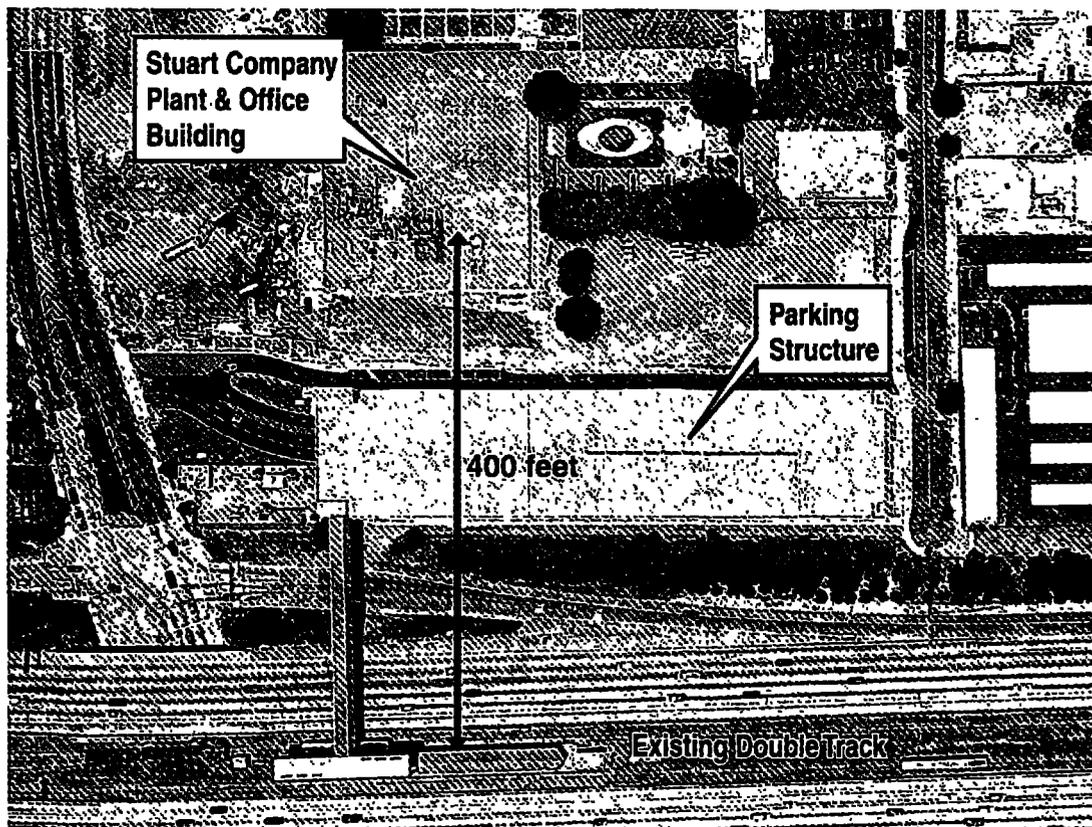


FIGURE 1

(Proposed project in relation to The Stuart Company Plant and Office Building)

Gold Line Foothill Extension – Pasadena to Montclair

Monrovia Santa Fe Depot, 1709 Myrtle Avenue, Monrovia

(In 2005, the parking structure formerly proposed to be south of the side platform option has been eliminated thereby reducing any unrelated proximity impacts on the Historic Monrovia Train Station).

The two proposed station platforms under the Build LRT to Azusa Alternative would be located in the ROW approximately 70 feet to the west of the historic depot. At this distance the two platforms would not obstruct views to the historic depot. The proposed project's station platforms would be approximately 3-4 feet in height and constructed with a waiting shelter/canopy, waiting benches, ticket kiosks and centenary wire support poles. While the construction of the new platforms would introduce a visual element, it would be of a scale and size that would not diminish the historic integrity of the Santa Fe Depot building and would be compatible with its historic use and setting as a passenger railroad depot. (see Figure 2)

The Monrovia Santa Fe Depot is currently being restored and will be incorporated into development plans.

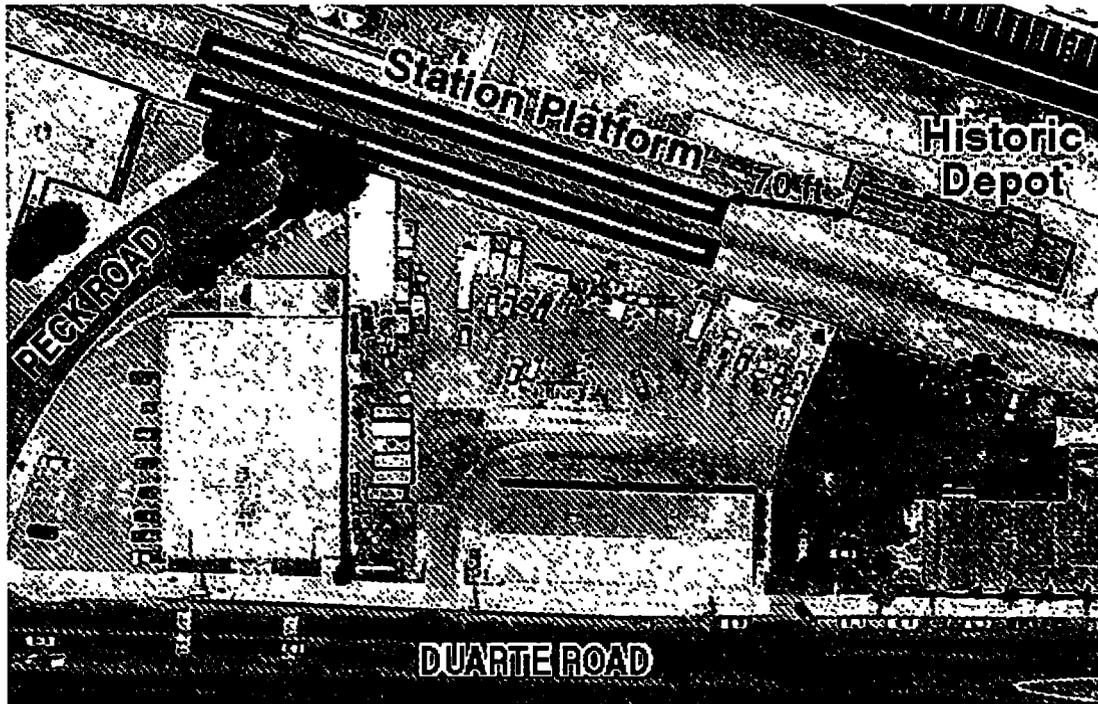


FIGURE 2

(Proposed project in relation to Monrovia Depot)

Gold Line Foothill Extension – Pasadena to Montclair

Temple Beth Hatikvah, City of Hope, 500 (north) Duarte Road, Duarte

(In 2005, the parking structure formerly proposed south of Duarte Road has been eliminated thereby reducing any related proximity impacts on the historic Temple Beth Hatikvah).

The proposed project's station platform would be located approximately 1,500 feet to the northeast of the Temple Beth Hatikvah, and the parking structure would be located approximately 1,400 feet to the northeast. Because of this great distance both proposed structures would not introduce any visual, atmospheric or audible elements, and therefore would not diminish the integrity of the property's significant historic features. Therefore there would be no effect for the proposed project under Section 106 *criteria example (v)*.

Visitor's Center, City of Hope, 1500 (south) Duarte Road, Duarte

(In 2005, the parking structure formerly proposed south of Duarte Road has been eliminated thereby reducing any related proximity impacts on the historic Visitor's Center).

The proposed project's station platform would be located approximately 1,800 feet to the northeast of the Visitor's Center, and the parking structure would be located approximately 1,580 feet to the northeast. Because of this great distance both proposed structures would not introduce any visual, atmospheric or audible elements, and therefore would not diminish the integrity of the property's significant historic features. Therefore there would be no effect for the proposed project under Section 106 *criteria example (v)*.

Gold Line Foothill Extension – Pasadena to Montclair

Atchison Topeka & Santa Fe Railroad Station (Azusa Santa Fe Railroad Depot), 129 East Santa Fe Avenue, Azusa

(In 2005, one of the proposed potential parking sites east of Alameda Avenue has been moved east of Azusa Avenue. An additional side platform has been added to the proposed 2004 platform).

The proposed project's station platforms under the Build LRT to Azusa Alternative would be located approximately 25 feet to the northeast of the historic AT&SF depot. The proposed project's station platforms would be approximately 3-4 feet in height and constructed with a waiting shelter/canopy, waiting benches, ticket kiosks and centenary wire support poles. While the construction of the new platforms would introduce a visual element it would be of a scale and size that would not diminish the historic integrity of the historic Azusa AT&SF depot building and would be compatible with its historic use and setting as a passenger railroad depot. The surface parking would be located approximately 100 feet to the north on a site that is currently occupied by light industrial warehouse type buildings, with two or three residential type structures located at the northern end. There would be no effect for the proposed project under Section 106 *criteria example (v)* (see Figure 3).

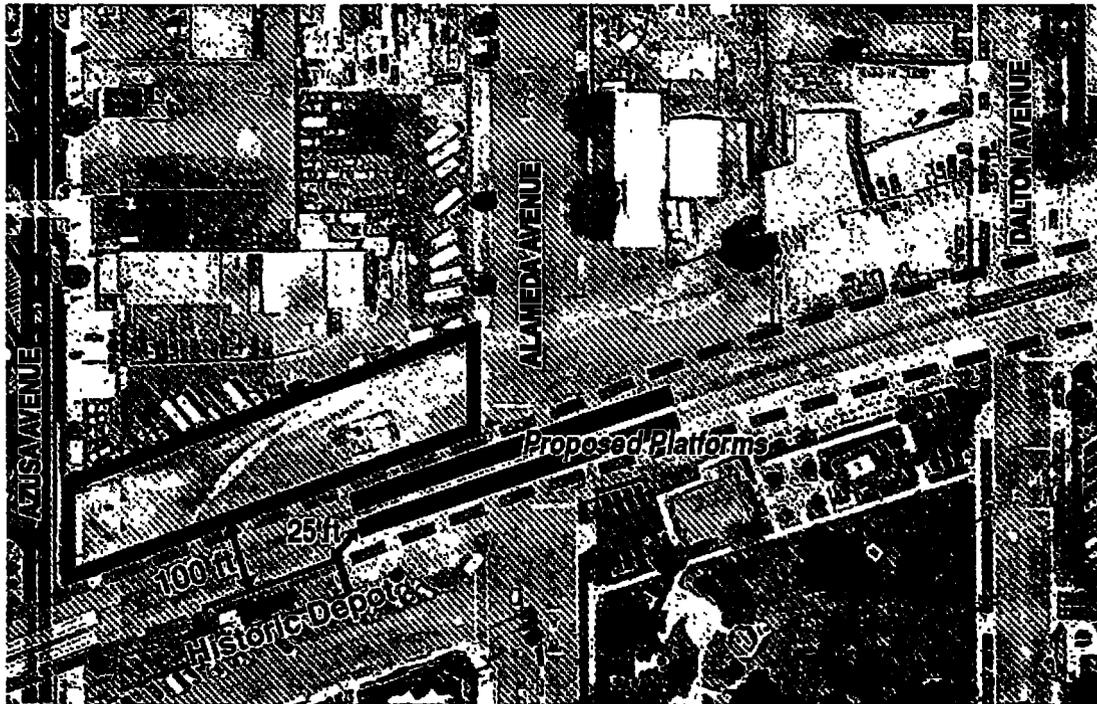


FIGURE 3

(Proposed project in relation to The Azusa AT&SF Railroad Depot)

Gold Line Foothill Extension – Pasadena to Montclair

Atchison Topeka & Santa Fe Depot (San Dimas Train Station Depot.) 210 W. 1st Street, San Dimas

(In 2005, the proposed parking and station options east of Eucla Avenue have been eliminated thereby reducing any related proximity on the Historic AT&SF Station).

For the proposed Full Build Alternative, the station platform would be approximately 2,000 feet to the northwest in the ROW and a three-level parking structure and surface parking, would be located approximately 2,000 feet to the northwest across the ROW. Because the proposed platform and parking structure would be at such an extreme distance from the historic depot building, there would be no potential to diminish the integrity of the historic depot building. The proposed project would have no adverse effect on the San Dimas AT&SF Depot under Section 106 *criteria example (v)*.

San Dimas Lemon Association Packing House (Machinery and Equipment Company, Inc.) 115 N. Cataract Avenue, San Dimas

(In 2005, the proposed parking and station options east of Eucla Avenue have been eliminated thereby reducing any related proximity on the San Dimas Lemon Association Packing House).

The proposed project's station platform, for the Full Build Alternative, would be located approximately 1,000 feet to the northwest in the ROW. Because of the great distance from the historic Lemon Association Packing House the proposed project would result in a finding of no effect under Section 106 *criteria example (v)*.

Gold Line Foothill Extension – Pasadena to Montclair

Santa Fe Pomona Station, 120 East Santa Fe Street, Pomona

(In 2005, investigation and research has noted a correction to this property, identified in our previous correspondence as the *Southern Pacific Station Depot*, located at 101 West First Street, Pomona, and eligible for the NRHP. The correct resource name and location is “Santa Fe Pomona Station,” located at 120 E. Santa Fe Street, Pomona. This resource is found not to be eligible for the NRHP and FTA seeks SHPO concurrence with this finding. The undertaking for 2005 has not changed for this resource and would not affect this building).

The proposed Pomona station platform would be approximately 850 feet to the west, in the ROW, and a 3-level parking structure would be located approximately 600 feet to the northwest. The proposed platform would be constructed at a great distance from the historic Santa Fe Pomona depot building and would not have the potential to impact the historic property. The proposed parking structure would also be constructed at great distance from the historic depot building and would be visually separated from it by a large existing warehouse building and parking lot. Therefore the proposed project would result in a finding of no effect under Section 106 *criteria example (v)* (see *Figure 4*).

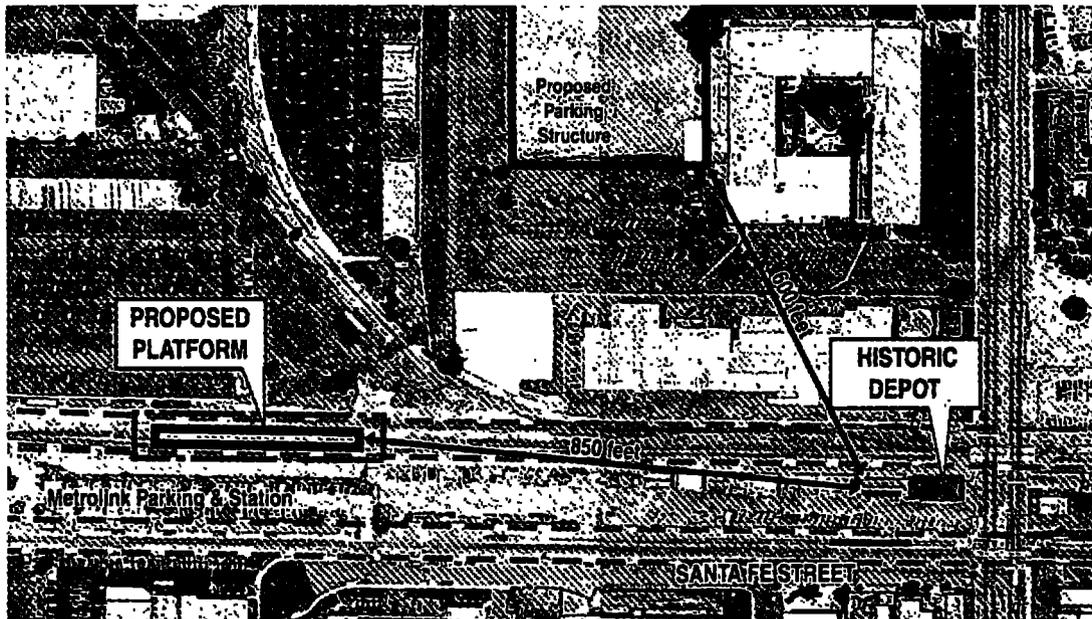


FIGURE 4

(Proposed project in relation to the Santa Fe Pomona Station)

Gold Line Foothill Extension – Pasadena to Montclair

Packing House (Corona College Heights Lemon Packing House), 510-532 W. 1st Street, Claremont

(In 2005, the 4-level parking structure formerly proposed to be west of Indian Hill Boulevard and north of the ROW has been eliminated thereby reducing any related proximity impacts on the Historic Packing House (see Figure 5).

The proposed LRT center platform would be approximately 850 feet to the west, in the ROW, and a proposed 3-level parking structure would be located approximately 600 feet to the northwest. Because of their distance, both proposed structures would not change the present use or diminish the integrity of the significant historic features of the Southern Pacific Station or its setting in any way and would result in a finding of no adverse effect on this historic property under Section 106 *criteria example (v)*.



FIGURE 5

Gold Line Foothill Extension – Pasadena to Montclair

Atchison Topeka & Santa Fe Railroad Station (Claremont Depot), 110 W. 1st Street, Claremont

(In 2005, the proposed 2004 Platforms have been eliminated and a proposed 2005 Platform has been added south of the Historic AT&SF Depot).

The proposed Claremont station sloped entrance walk and platform would be located approximately 25 feet to the south and southwest of the historic Atchison Topeka & Santa Fe Railroad Station. The proposed project's station platforms would be approximately 3-4 feet in height and constructed with a waiting shelter/canopy, waiting benches, ticket kiosks and centenary wire support poles. While the construction of the new sloped entrance walk and platforms would introduce a visual element it would be of a scale and size that would not diminish the historic integrity of the historic Atchison Topeka & Santa Fe Railroad Station and would be compatible with its historic use and setting as a passenger railroad depot. A two level parking structure would be constructed approximately 930 feet to the east of the Claremont Depot. The parking structure would be sufficiently distanced from the historic depot building as to not visually impact it in any way. Therefore the proposed project would result in a finding of no effect under Section 106 *criteria example (v)* (see *Figure 6*).

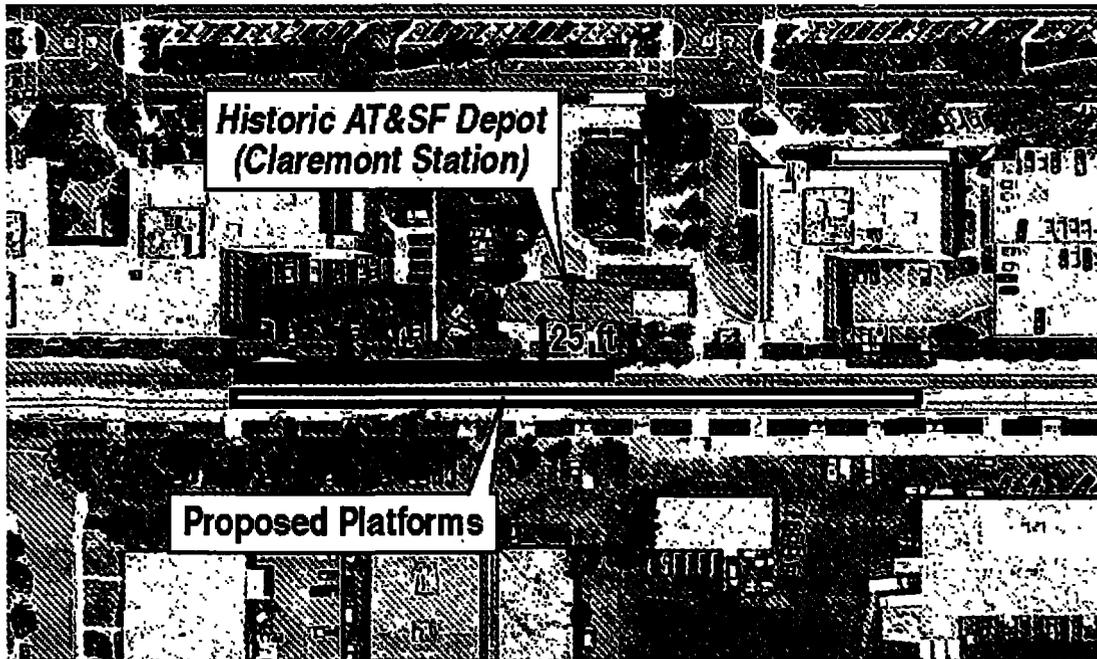


FIGURE 6

(Proposed project in relation to the Claremont AT&SF Depot)

Gold Line Foothill Extension – Pasadena to Montclair

Sumner House, 105 North College Avenue, Claremont

The proposed Claremont station platform would be located over 600 feet to the west of the Sumner House, and would be visually separated from it by First Street and a large commercial building. A two level parking structure would be constructed on an existing surface parking lot located at the southeast corner of College Avenue and First Street, approximately 140 feet from the Sumner House. The proposed parking structure would only be two stories in height, would be located within the boundaries of an existing surface parking lot, and would be separated from the Sumner House by the intersection of College Avenue and First Street, and would therefore not visually impact it in any way. Therefore the proposed project would result in a finding of no effect under Section 106 *criteria example (v)* (see Figure 7).

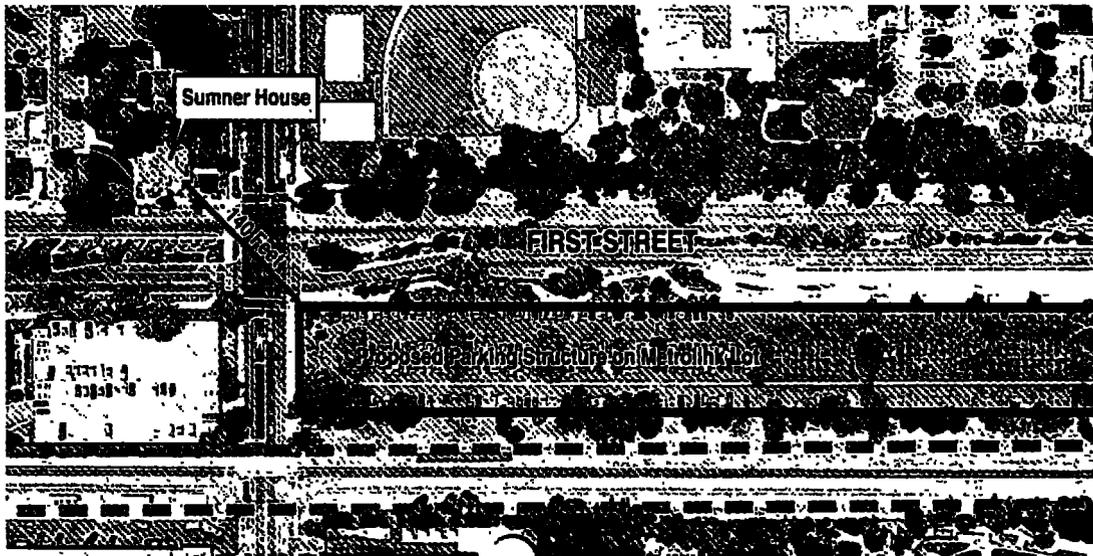


FIGURE 7

(Proposed project in relation to the Sumner House)

Finding of No Adverse Effect

No known historic properties would be damaged as a result of the proposed project, however cultural resources are known to exist within or adjacent to the Foothill Extension Project updated (2005) APE, and there is a potential for unknown subsurface structural remains or prehistoric sites within the Project APE. If during construction archeological properties are discovered, the steps outlined in 36 CFR 800.13 (b) would be followed, unless FTA, SHPO, and the Council Choose to enter into a Memorandum of Agreement for this undertaking.

The proposed Build LRT to Azusa Alternative and Full Build Alternative would result in a finding of *no effect* under Section 106 for all properties within the project updated (2005) APE. Therefore, there would be no change in the previous finding of *no adverse effect* under Section 106, and FTA requests your concurrence with this finding.

Gold Line Foothill Extension – Pasadena to Montclair

Bibliography

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Letter from Vincent L. Mas, Acting Director of Planning, City of Irwindale, November 18, 2003

Letter from Suzane Cole, Associate Planner, Community Development Department, City of Azusa, January 22, 2004

Letter from David Chantarangsu, AICP, Assistant Director of Planning, City of Glendora

Letter from Cathie Chavez, San Gabriel and Lower Los Angeles Rivers and Mountains Conservancy, December 19, 2003

Archeological Information Center – San Bernardino County Museum, Laska, Robin E, Assistant Center Coordinator. Letter received on October 7, 2003, stating that a Historical Resources Record Search was conducted on USGS Ontario 7.5' quad.

South Coastal Information Center, California State, Fullerton, St. James, Stacy, Assistant Coordinator, letter received on October 15, 2003 stating that a historic and prehistoric archeological sites record search was conducted within a half mile of the project area.

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State of California, Office of Historic Preservation, *California Register of Historical Resources*

State of California, Office of Historic Preservation *Historical Resources Inventory System*

State of California, Office of Historic Preservation, *California Historical Landmarks*

State of California, Office of Historic Preservation, *California Points of Historical Interest*

City of Claremont, *Historic-Cultural Monuments list*

City of La Verne, *Planning Department files*

List of Preparers, Architectural History

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B.A. Architecture. Over 4 years and a half of experience at MFA/Jones & Stokes in the identification of historic buildings in the city of Los Angeles for proposed Historic Preservation Overlay Zones.

Carrie Chasteen, Architectural Historian - Historical Research and Site Evaluations

M.S. Historic Preservation. Over three years of experience at MFA/Jones & Stokes in Section 106 compliance and architectural/historic surveys.

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Architecture. Over 20 years of experience in documenting historical resources, resulting in 10 City of Los Angeles Historic Preservation Overlay Zones and 50 City of Los Angeles Historic-Cultural Monuments.

Jessica Feldman, Architectural Historian - Bridge Evaluations

M.A. Historic Preservation Planning. Over 7 years of cultural resources experience, including historic surveys, determination of eligibility, and preparation of historic property inventory forms.

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Ph.D. (Candidate) American Culture Studies. Over two years of experience at MFA/Jones & Stokes conducting library, microfiche and Internet research, property research, field surveys, and photographic documentation.

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Over 6 years of cultural resources experience, including historic surveys, determinations of eligibility, CEQA analyses, and mitigation options.

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Primary # _____
HR # _____
Trinomial _____
CHRC Status Code 6Z Pending SHPO concurrence

PRIMARY RECORD

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2

* Resource Name or #: AT & SF Railroad bridge over Huntington Drive

P1. Other Identifier: Huntington Drive bridge

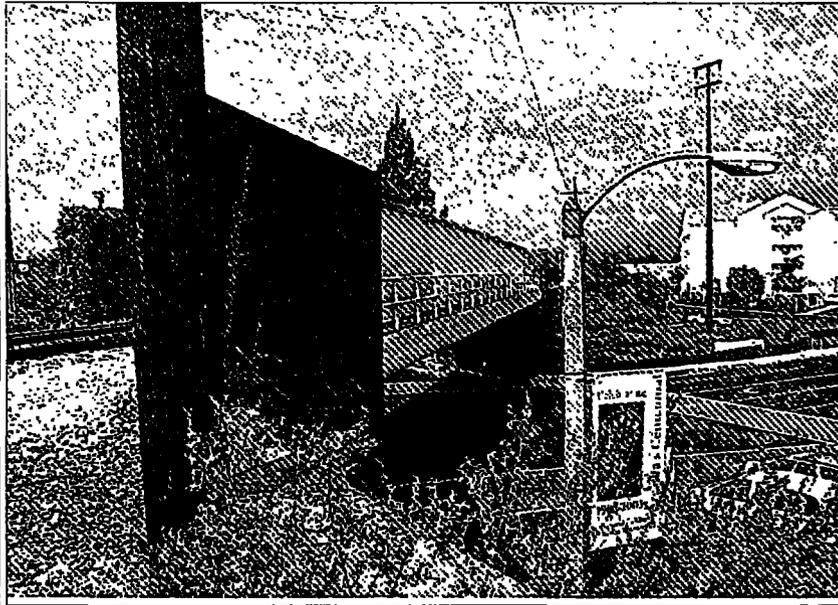
* P2. Location: Not for Publication Unrestricted a. County Los Angeles
b. USGS 7.5' Quad _____ Date _____ T _____; R _____; _____ 1/4 of _____ 1/4 of Sec _____; _____ B.M.
c. Address _____ City Arcadia Zip _____
d. UTM: (Give more than one for large and/or linear feature) Zone _____, _____ mE/ _____ mN
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM, etc. as app
Mile Post 123.800

* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
The Huntington Drive bridge is a riveted plate, or thru, girder. It was constructed by the railroad in 1942. The end abutments are brick construction. There is a two-foot wide walkway with a two rail metal baluster on the southern side of the bridge.

* P3b. Resource Attributes: (List attributes and codes) HP19 Bridge

* P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking along the bridge's south side from west to east

* P6. Date Constructed/Age and Sources:

Prehistoric Historic Both

1942 Constructed

* P7. Owner and Address:

Southern CA Regional Rail Auth.

P--Private

* P8. Recorded by: (Name, affiliation, address)

Jessica B. Feldman

Myra Frank & Assoc./Jones & Stokes

811 W. 7th Street, Suite 800

Los Angeles, CA 90017

* P9. Date Recorded: 2/18/2004

* P10. Survey Type: (Describe)

Intensive Survey Effort

Section 106 Compliance

P--Project Review

* P11. Report Citation: (Cite survey report/other sources or "none") Gold Line Phase II -- Foothill Extension

Supplemental Historic Property Survey and Effects Report, June 2005

* Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record
 Photograph Record Other: (List) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

* NRHP Status Code 6Z Pending SHPO concurrence

* Resource Name or #: AT & SF Railroad bridge over Huntington Drive

B1 Historic Name: _____

B2. Common Name Huntington Drive bridge

B3. Original Use: Bridge B4. Present Use: Bridge

* B5. Architectural Style: _____

* B6. Construction History: (Construction date, alterations, and date of alterations)
This bridge was constructed in 1942, using design "E72". The brick bridge abutments appear to have been rebuilt at a later date.

* B7. Moved? No Yes Unknown Date _____ Original Location: _____

* B8. Related Features: _____

B9a. Architect: ATSF Railway b. Builder: Unknown

* B10. Significance: Theme Railroad bridges Area Pasadena

Period of Significance 1942 Property Type Bridge Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity)

The Huntington Drive bridge, a single track railroad bridge over Huntington Drive in Arcadia, California, is a riveted plate, or thru, girder bridge. The thru girder bridge has been used for railroad bridges since the mid-19th century and is one of the most common designs still in use throughout the rail system in the United States. As is typical with most thru girder bridges, this bridge is plain and unadorned. It appears that the brick bridge abutments were constructed at a later date, and no historical photographs were obtained which showed the design of the original abutments. Therefore, in addition to any general maintenance that may have replaced elements of the bridge, such as the timber ties, the new abutments contribute to a loss of integrity of design, materials, workmanship and feeling. Therefore, this bridge does not appear eligible for the National Register of Historic Places under Criterion C or the California Register of Historical Resources under Criterion 3. There are no known important persons or events associated with this bridge; therefore it is not eligible for the National Register under Criteria A or B or the California Register under Criteria 1 or 2.

B11. Additional Resource Attributes: (List attributes and codes): _____

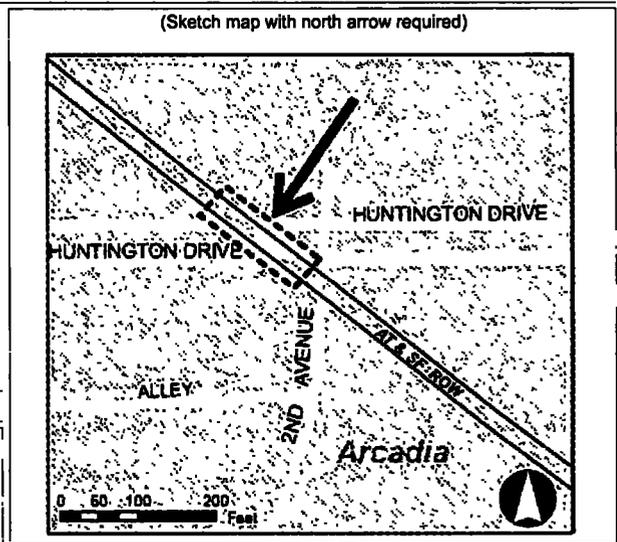
* B12. References:
The Chief Way Reference Series System Standards, 1978

Duke, Donald and Stan Kistler, Santa Fe... Steel Rails Through California, 1963.

B13. Remarks: _____

* B14. Evaluator: Jessica B. Feldman
Date of Evaluation: 2/2/2004

(This space reserved for official comments.)



Primary # _____
HR # _____
Trinomial _____
CHRC Status Code 5S2 Pending SHPO concurrence
Other Listings _____
Review Code _____ Reviewer _____ Date _____

PRIMARY RECORD

Page 1 of 2

* Resource Name or #: 5th Avenue Pedestrian Tunnel under ATSF Railroad, Monrovia

P1. Other Identifier: 5th Avenue Pedestrian Underpass

* P2. Location: Not for Publication Unrestricted a. County Los Angeles

b. USGS 7.5' Quad _____ Date _____ T _____; R _____; _____ 1/4 of _____ 1/4 of Sec _____; _____ B.M.

c. Address _____ City Monrovia Zip _____

d. UTM: (Give more than one for large and/or linear feature) Zone _____, _____ mE/ _____ mN

e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM's, etc. as app
Mile Post 123.500

* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
The 5th Avenue Pedestrian Underpass is a concrete reinforced box, similar to a large culvert, situated beneath the railroad tracks in the City of Monrovia. The opening has chamfered corners and measures six feet wide by eight feet high. Each side of the opening is sheltered by a stepped concrete retaining wall. An eight inch wide fire (water) main runs along the bottom east side of the tunnel. The tunnel appears to be in good condition and receives regular inspections.

* P3b. Resource Attributes: (List attributes and codes) HP11 Engineering structure

* P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking north at the south tunnel opening

* P6. Date Constructed/Age and Sources:

Prehistoric Historic Both

1942 Constructed

* P7. Owner and Address:

Southern CA Regional Rail Auth.

P--Private

* P8. Recorded by: (Name, affiliation, address)

Jessica B. Feldman

Myra Frank & Assoc./Jones & Stokes

811 W. 7th Street, Suite 800

Los Angeles, CA 90017

* P9. Date Recorded: 2/18/2004

* P10. Survey Type: (Describe)

Intensive Survey Effort

Section 106 Compliance

P--Project Review

* P11. Report Citation: (Cite survey report/other sources or "none") Gold Line Phase II -- Foothill Extension

Supplemental Historic Property Survey and Effects Report, June 2005

* Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record

Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record

Photograph Record Other: (List) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

* NRHP Status Code 5S2Pending SHPO concurrence

* Resource Name or #: 5th Avenue Pedestrian Tunnel under ATSF Railroad, Monrovia

B1. Historic Name: _____

B2. Common Name 5th Avenue Pedestrian Undercrossing

B3. Original Use: Pedestrian Tunnel B4. Present Use: Pedestrian Tunnel

* B5. Architectural Style: None

* B6. Construction History: (Construction date, alterations, and date of alterations.)

The tunnel was constructed in 1942. It was repainted in 2001.

* B7. Moved? No Yes Unknown Date _____ Original Location: _____

* B8. Related Features:

B9a. Architect: Unknown b. Builder: Unknown

* B10. Significance: Theme Railroad bridges Area Monrovia

Period of Significance 1942 Property Type Pedestrian Tunnel Applicable Criteria 1

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The 5th Avenue Pedestrian Undercrossing was constructed in 1942 and appears to be unaltered, except for occasional painting and the installation of a water main along the bottom of one interior wall. The integrity of its location, setting, materials, workmanship, feeling and association have not been compromised. The construction of a railroad culvert used specifically for pedestrian use is rare, and not addressed in the common standard plan books for most railroad companies. The history of the construction of this particular tunnel is not known at this time, but it seems appropriate that this tunnel might have local significance. Therefore, it appears eligible for the California Register under Criterion 1, for its association with an event that has made a significant contribution to the broad patterns of local or regional history. No known persons or events of national significance appear associated with this structure. Therefore it does not appear eligible for the National Register of Historic Places under Criteria A or B, nor does it appear eligible for the California Register of Historical Resources under Criterion 2. The design of the tunnel is not unusual or ornate; it does not appear to represent any characteristics or a type or period, nor does it represent the work of a master. It does not possess high artistic value. Therefore, it is not eligible for the National Register of Historic Places under Criterion C, or under Criterion 3 of the California Register of Historical Resources.

B11. Additional Resource Attributes: (List attributes and codes) _____

* B12. References:

The Chief Way Reference Series System Standards, 1978

B13. Remarks:

* B14. Evaluator: Jessica B. Feldman

Date of Evaluation: 2/2/2004

(This space reserved for official comments.)

(Sketch map with north arrow required)



Primary # _____
HR # _____
Trinomial _____
CHRC Status Code 6Z Pending SHPO concurrence

PRIMARY RECORD

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2

* Resource Name or #: AT & SF Railroad bridge over Alta Vista Wash

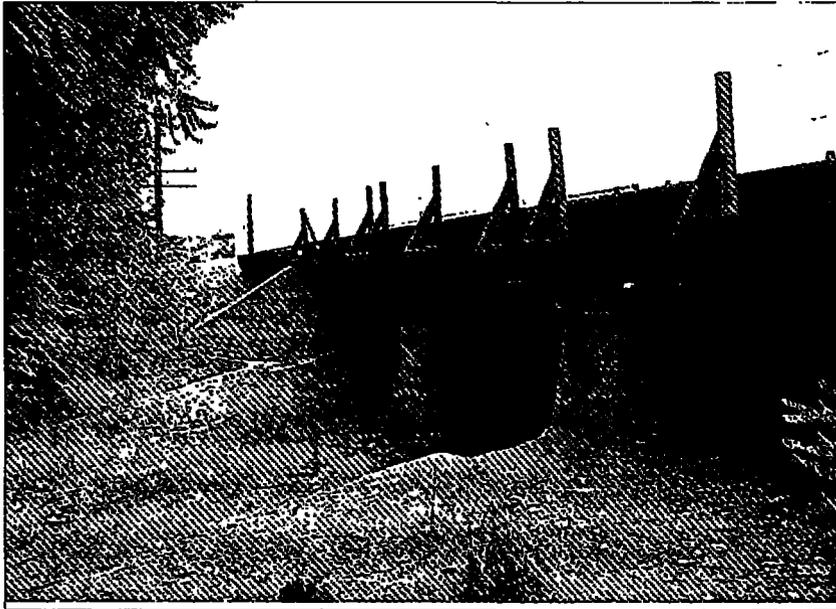
P1. Other Identifier: Alta Vista Wash Deck Beam Bridge

* P2. Location: Not for Publication Unrestricted a. County Los Angeles
b. USGS 7.5' Quad _____ Date _____ T. _____; R. _____; 1/4 of _____ 1/4 of Sec. _____; B.M. _____
c. Address _____ City Monrovia Zip _____
d. UTM: (Give more than one for large and/or linear feature) Zone _____, _____ mE/ _____ mN
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM's, etc. as app
Mile Post 122.800

* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
The deck beam bridge carrying the former Atchison, Topeka and Santa Fe railroad over the Alta Vista Wash in Monrovia, California, measures 60 feet long and 15 feet wide. The bridge is actually three spans, measuring 20 feet each. The bridge rests on angled, smooth concrete abutments and two squat concrete piers, one of which rests in the dry wash. The wash is channelized. The bridge was constructed on four steel I-beams with two-foot walkways, and has a timber ballast trough. The sidewalks have no railings, but there are exterior wood brackets. Routine maintenance has indicated the need to remove and replace numerous ties, that the membrane is deteriorating and that the bridge has been vandalized by graffiti. According to engineering reports, the bridge is not considered to be structurally sound for heavy loads.

* P3b. Resource Attributes: (List attributes and codes) HP19 Bridge
* P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)

Looking west along the south side of the bridge

* P6. Date Constructed/Age and Sources:
 Prehistoric Historic Both
1907 Constructed

* P7. Owner and Address:
Southern CA Regional Rail Auth.

P--Private

* P8. Recorded by: (Name, affiliation, address)
Jessica B. Feldman
Myra Frank & Assoc./Jones & Stokes
811 W. 7th Street, Suite 800
Los Angeles, CA 90017

* P9. Date Recorded: 2/21/2004

* P10. Survey Type: (Describe)
Intensive Survey Effort
Section 106 Compliance
P--Project Review

* P11. Report Citation: (Cite survey report/other sources or "none") Gold Line Phase II -- Foothill Extension
Supplemental Historic Property Survey and Effects Report, June 2005

* Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record
 Photograph Record Other: (List) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

* NRHP Status Code 6Z Pending SHPO concurrence

* Resource Name or #: AT & SF Railroad bridge over Alta Vista Wash

B1. Historic Name _____

B2. Common Name Alta Vista Wash Deck Beam Bridge

B3. Original Use: Bridge B4. Present Use: Bridge

* B5. Architectural Style: _____

* B6. Construction History: (Construction date, alterations, and date of alterations.)

This bridge was constructed in 1907. Identified on maintenance logs as design "OSL".

* B7. Moved? No Yes Unknown Date _____ Original Location: _____

* B8. Related Features:

B9a. Architect: ATSF Railway b. Builder: Unknown

* B10. Significance: Theme Railroad bridges Area Monrovia

Period of Significance 1907 Property Type Bridge Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Alta Vista Wash Deck Beam Bridge is a single track railroad bridge. The deck beam bridge style has been used for railroad bridges since the mid-19th century and is one of the more common designs in use throughout the rail system in the United States. It is not an exceptional example of a particular railroad bridge design, it is not significantly long, nor does it display the use of an advanced engineering design. It appears likely that part of the original railing has been removed and it shows signs of deterioration and vandalism. Therefore, in addition to any general maintenance that may have replaced elements of the bridge, such as the timber ties, the bridge has lost integrity of materials, and workmanship. This bridge does not appear eligible for the National Register of Historic Places under Criterion C or the California Register of Historical Resources under Criterion 3. There are no known important persons or events associated with this bridge; therefore it is not eligible for the National Register under Criteria A or B or the California Register under Criteria 1 or 2.

B11. Additional Resource Attributes. (List attributes and codes): _____

* B12. References:

The Chief Way Reference Series System Standards, 1978

Duke, Donald and Stan Kistler, Santa Fe...Steel Rails Through California, 1963.

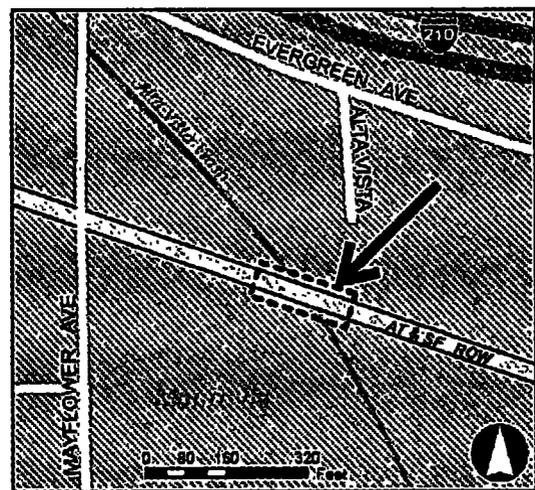
B13. Remarks:

* B14. Evaluator: Jessica B. Feldman

Date of Evaluation: 2/20/2004

(This space reserved for official comments)

(Sketch map with north arrow required)



Primary # _____
HR # _____
Trinomial _____
CHRC Status Code 6Z pending SHPO concurrence

PRIMARY RECORD

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2

* Resource Name or #: AT & SF Railroad bridge over Santa Anita Wash

P1. Other Identifier: Santa Anita Wash bridge

* P2. Location: Not for Publication Unrestricted a. County Los Angeles

b. USGS 7.5' Quad _____ Date _____ T _____; R _____; _____ 1/4 of _____ 1/4 of Sec _____; _____ B.M.

c. Address _____ City Monrovia Zip _____

d. UTM: (Give more than one for large and/or linear feature) Zone _____, _____ mE/ _____ mN

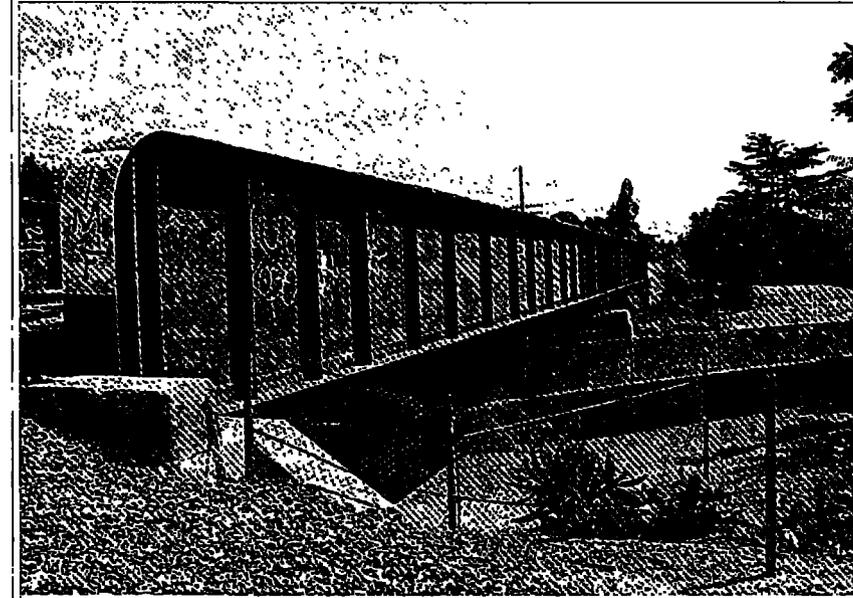
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM, etc. as app
Mile Post 123.300

* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)
The railroad bridge over the Santa Anita Wash is a two-span riveted plate, or thru, girder. The total span measurement just over 180 feet in length, and has a width of 18 feet. Each span measures approximately 90 feet, and they meet in the middle at a concrete pier. The bridge span ends rest on smooth concrete abutments. The sides of the bridge are steel plate girders. A single track is carried by the bridge over the Santa Anita Wash. The bridge is in poor condition according to recent maintenance records: the ballast is low throughout, the membrane is bad, bolts are missing, the bridge dips at the center pier and there is some fire damage on the east end. Additionally, the bridge is marred by graffiti.

* P3b. Resource Attributes: (List attributes and codes) HP19 Bridge

* P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b Description of Photo: (View, date, etc.)

Looking west to east along the northern side.

* P6. Date Constructed/Age and Sources:
 Prehistoric Historic Both

1915 Constructed

* P7. Owner and Address:
Southern CA Regional Rail Auth.

P--Private

* P8. Recorded by: (Name, affiliation, address)
Jessica B. Feldman
Myra Frank & Assoc./Jones & Stokes
811 W. 7th Street, Suite 800
Los Angeles, CA 90017

* P9. Date Recorded: 2/18/2004

* P10. Survey Type: (Describe)
Intensive Survey Effort
Section 106 Compliance
P--Project Review

* P11. Report Citation: (Cite survey report/other sources or "none") Gold Line Phase II -- Foothill Extension
Supplemental Historic Property Survey and Effects Report, June 2005

* Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record
 Photograph Record Other: (List) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

* NRHP Status Code 6Z pending SHPO concurrence

* Resource Name or #: AT & SF Railroad bridge over Santa Anita Wash

B1. Historic Name: _____

B2. Common Name Santa Anita Wash bridge

B3. Original Use: Bridge B4. Present Use: Bridge

* B5. Architectural Style: _____

* B6. Construction History: (Construction date, alterations, and date of alterations.)
This bridge was constructed in 1915 using design "E72".

* B7. Moved? No Yes Unknown Date _____ Original Location: _____

* B8. Related Features:

B9a. Architect: ATSF Railway b. Builder: Unknown

* B10. Significance: Theme Railroad bridges Area Monrovia

Period of Significance 1915 Property Type Bridge Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Santa Anita Wash bridge, a single track railroad bridge over the Santa Anita Wash in Monrovia, California, is a riveted plate, or thru, girder bridge. It was constructed in 1915, and is one of the earlier twentieth-century bridges on the ATSF line through Southern California. The thru girder bridge has been used for railroad bridges since the mid-19th century and is one of the most common designs still in use throughout the rail system in the United States. As is typical with most thru girder bridges, this bridge is plain and unadorned. In addition, it is in poor condition and has been vandalized. The bridge has lost integrity of materials and workmanship, and is not remarkable for its design, length, or date of construction. Therefore, this bridge does not appear eligible for the National Register of Historic Places under Criterion C or the California Register of Historical Resources under Criterion 3. This bridge is an example of a common railroad bridge design, and there are numerous examples still in use on this rail corridor. In addition, no known persons or events identified as important in local, state or national history were determined to be associated with this bridge. Therefore it is not eligible for the National Register under Criteria A or B or the California Register under Criteria 1 or 2.

B11. Additional Resource Attributes: (List attributes and codes): _____

* B12. References:
The Chief Way Reference Series System Standards, 1978

Duke, Donald and Stan Kistler, Santa Fe...Steel Rails Through California, 1963.

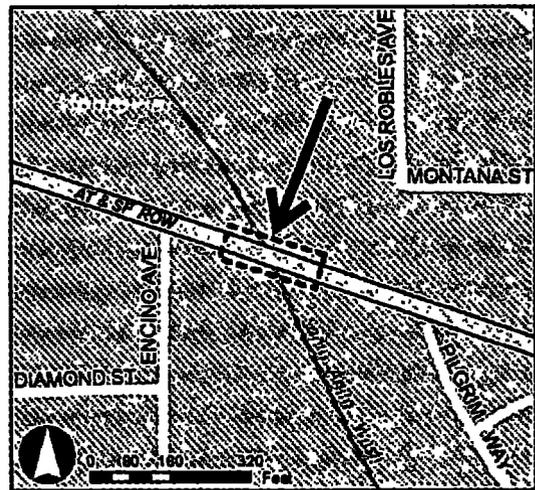
B13. Remarks:

* B14. Evaluator: Jessica B. Feldman

Date of Evaluation: 2/2/2004

(This space reserved for official comments.)

(Sketch map with north arrow required)



Primary # _____
HR # _____
Trinominal _____
CHRC Status Code 6Z Pending SHPO concurrence

PRIMARY RECORD

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2

* Resource Name or #: AT & SF Railroad bridge over Sawpit Wash

P1. Other Identifier: Sawpit Wash bridge

* P2. Location: Not for Publication Unrestricted a. County Los Angeles

b. USGS 7.5' Quad _____ Date _____ T _____; R _____; 1/4 of _____ 1/4 of Sec _____; B.M. _____

c. Address _____ City Monrovia Zip _____

d. UTM: (Give more than one for large and/or linear feature) Zone _____, _____ mE/ _____ mN

e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTMs, etc. as app
Mile Post 121.700

* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)
The single track railroad bridge over the Sawpit Wash in Monrovia, California, is a riveted steel plate girder, also known as a thru girder. The main span measures 60 feet in length and is 18 feet wide with two-foot sidewalks on both sides. The bridge also has a timber ballast trough. There are two 14 foot long approach spans constructed of timber with wood railings. The steel girders have been vandalized with graffiti and maintenance records indicate that minor repairs are necessary to the ballast.

* P3b. Resource Attributes: (List attributes and codes) HP19 Bridge

* P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects) P5b. Description of Photo: (View, date, etc.)



Looking west along the south side of the bridge.

* P6. Date Constructed/Age and Sources:
 Prehistoric Historic Both

1941 Constructed

* P7. Owner and Address:
Southern CA Regional Rail Auth.

P--Private

* P8. Recorded by: (Name, affiliation, address)
Jessica B. Feldman

Myra Frank & Assoc./Jones & Stokes
811 W. 7th Street, Suite 800
Los Angeles, CA 90017

* P9. Date Recorded: 2/18/2004

* P10. Survey Type: (Describe)
Intensive Survey Effort
Section 106 Compliance

P--Project Review

* P11. Report Citation: (Cite survey report/other sources or "none") Gold Line Phase II -- Foothill Extension
Supplemental Historic Property Survey and Effects Report, June 2005

* Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record
 Photograph Record Other: (List) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

* NRHP Status Code 6Z Pending SHPO concurrence

* Resource Name or #: AT & SF Railroad bridge over Sawpit Wash

B1. Historic Name: _____

B2. Common Name Sawpit Wash bridge

B3. Original Use. Bridge B4. Present Use. Bridge

* B5. Architectural Style: _____

* B6. Construction History: (Construction date, alterations, and date of alterations)

This bridge was constructed in 1941 using design "E72".

* B7. Moved? No Yes Unknown Date _____ Original Location: _____

* B8. Related Features:

B9a. Architect: ATSF Railway b. Builder: Unknown

* B10. Significance: Theme Railroad bridges Area Duarte

Period of Significance 1941 Property Type Bridge Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Sawpit Wash bridge, a single track railroad bridge over the Sawpit Wash in Duarte, California, is a riveted plate, or thru, girder bridge. It was constructed in 1941. The thru girder bridge has been used for railroad bridges since the mid-19th century and is one of the most common designs still in use throughout the rail system in the United States. As is typical with most thru girder bridges, this bridge is plain and unadorned, although it has two timber approach spans which are not a typical thru-girder bridge element. The bridge is not of extraordinary length, it is not one of the earlier thru girder bridges and over time the bridge has lost integrity of materials and workmanship. Therefore, this bridge does not appear eligible for the National Register of Historic Places under Criterion C or the California Register of Historical Resources under Criterion 3. This bridge was is an example of a common standard plan bridge, and there are numerous examples along the existing rail corridor. There are no known important persons or events associated with this bridge; therefore it is not eligible for the National Register under Criteria A or B or the California Register under Criteria 1 or 2.

B11. Additional Resource Attributes: (List attributes and codes): _____

* B12. References:

The Chief Way Reference Series System Standards, 1978

Duke, Donald and Stan Kistler, Santa Fe...Steel Rails Through California, 1963.

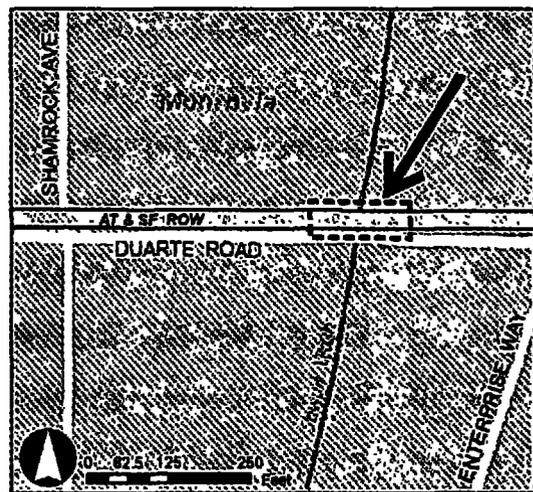
B13. Remarks:

* B14. Evaluator: Jessica B. Feldman

Date of Evaluation: 2/2/2004

(This space reserved for official comments.)

(Sketch map with north arrow required)



Primary # _____
HR # _____
Trinomial _____
CHRC Status Code 6Z Pending SHPO concurrence

PRIMARY RECORD

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2

* Resource Name or #: AT & SF Railroad bridge over San Gabriel River in Irwindale

P1. Other Identifier: San Gabriel River bridge

* P2. Location: Not for Publication Unrestricted a. County Los Angeles

b. USGS 7.5' Quad _____ Date _____ T _____; R _____; _____ 1/4 of _____ 1/4 of Sec _____; _____ B.M.

c. Address _____ City Irwindale Zip _____

d. UTM: (Give more than one for large and/or linear feature) Zone _____, _____ mE/ _____ mN

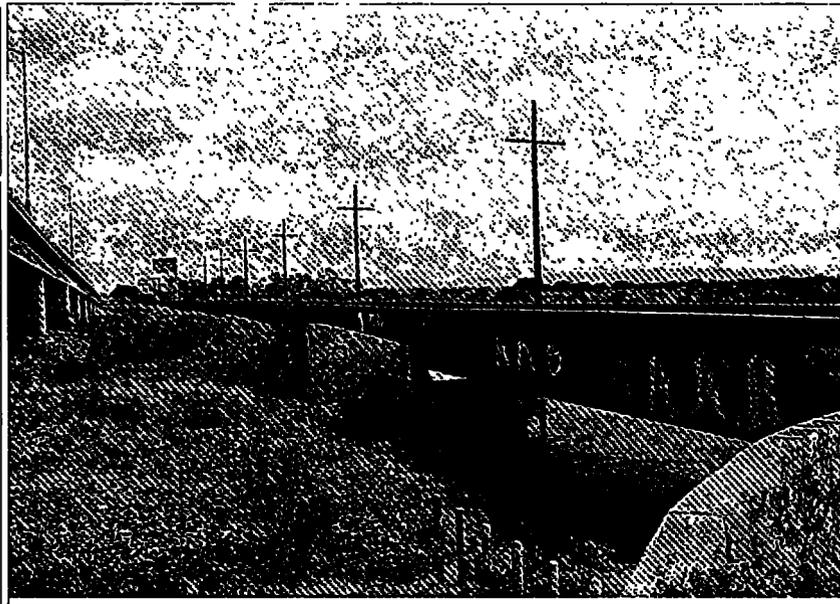
e. Other Locational Data: (e.g. parcel #, legal description, directions to resource, elevation, additional UTM's, etc. as app
Mile Post 119.400

* P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)
The railroad bridge over the San Gabriel River is a single-track bridge measuring over 700 feet in length. This riveted plate girder is 18 feet wide. The bridge is segmented into seven spans of equal length, with the ends of each span meeting at a concrete pier. The bridge seats, or piers, rest in the water. The steel plate girders have been vandalized. The bridge is considered structurally sound; all timber ties have been replaced.

* P3b. Resource Attributes: (List attributes and codes) HP19 Bridge

* P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects)



P5b. Description of Photo: (View, date, etc.)
Looking east along the north side of the bridge

* P6. Date Constructed/Age and Sources:
 Prehistoric Historic Both
1903 Constructed

* P7. Owner and Address:
Southern CA Regional Rail Auth.

P--Private

* P8. Recorded by: (Name, affiliation, address)
Jessica B. Feldman
Myra Frank & Assoc./Jones & Stokes
811 W. 7th Street, Suite 800
Los Angeles, CA 90017

* P9. Date Recorded: 2/18/2004

* P10. Survey Type: (Describe)
Intensive Survey Effort
Section 106 Compliance
P--Project Review

* P11. Report Citation: (Cite survey report/other sources or "none") Gold Line Phase II -- Foothill Extension
Supplemental Historic Property Survey and Effects Report, June 2005

* Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record
 Photograph Record Other. (List) _____

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

* NRHP Status Code 6Z Pending SHPO concurrence

* Resource Name or #: AT & SF Railroad bridge over San Gabriel River in Irwindale

B1. Historic Name: _____

B2. Common Name San Gabriel River bridge

B3. Original Use: Bridge B4. Present Use: Bridge

* B5. Architectural Style: _____

* B6. Construction History: (Construction date, alterations, and date of alterations.)
This bridge was construct in 1903, using design "E55"

* B7. Moved? No Yes Unknown Date _____ Original Location: _____

* B8. Related Features:

B9a. Architect: ATSF Railway b. Builder: Unknown

* B10. Significance: Theme Railroad bridges Area Irwindale

Period of Significance 1903 Property Type Bridge Applicable Criteria N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The single track railroad bridge over the San Gabriel River near Irwindale, California, is a seven span riveted plate, or thru, girder bridge. It was constructed in 1903 and is an early 20th century thru bridge on the ATSF line through Southern California. The thru girder bridge has been used for railroad bridges since the mid-19th century and is one of the most common designs still in use throughout the rail system in the United States. As is typical with most thru girder bridges, this bridge has no ornamentation. However, it is noteworthy for its length of 700 feet given its date of construction. It likely pre-dates the sluices near the western end of the bridge. The remarkable length of the bridge in light of its 1903 construction is less common among plate girder bridges, but not rare and over time the bridge has lost of integrity of materials and workmanship. Additionally, the construction of Interstate 210 just to the north of the bridge has diminished the integrity of the bridge's setting and feeling. Therefore, this bridge does not appear eligible for the National Register of Historic Places under Criterion C or the California Register of Historical Resources under Criterion 3. This bridge is an example of a common design, and there are numerous examples still in use along the rail corridor. There are no known important persons or events associated with this bridge; therefore it is not eligible for the National Register under Criteria A or B or the California Register under Criteria 1 or 2.

B11. Additional Resource Attributes: (List attributes and codes): _____

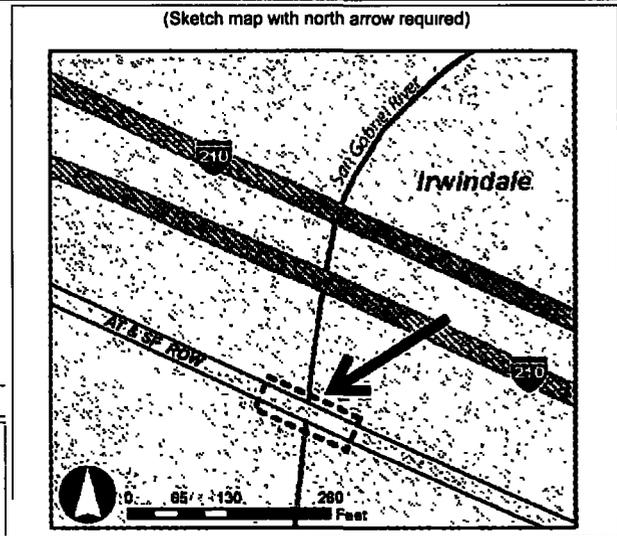
* B12. References:
The Chief Way Reference Series System Standards, 1978

Duke, Donald and Stan Kistler, Santa Fe...Steel Rails Through California, 1963.

B13 Remarks:

* B14. Evaluator: Jessica B. Feldman
Date of Evaluation: 2/2/2004

(This space reserved for official comments.)





John A. Sims, CP
Paralegal
Law Department

BNSF Railway Company
2500 Lou Menk Drive – AOB-3
Fort Worth, Texas 76131-2828
tel 817-352-2378
fax 817-352-2397
Email – john.sims@bnsf.com

April 29, 2011

Milford Wayne Donaldson, FAIA, State Historic Preservation Officer
California State Parks
Office of Historic Preservation
1725 23rd Street, Suite 100
Sacramento, CA 95816

**Re: STB Docket No. AB-6 (Sub-No. 477X) BNSF Railway Company –
Abandonment Exemption – in Los Angeles County, California**

BNSF Railway Company ("BNSF") anticipates filing in the near future a Notice of Exemption seeking Surface Transportation Board ("STB") authority in the above-referenced docket to abandon 4.85 miles of rail line in Los Angeles County, California, beginning at Milepost 124.20, just east of the Santa Anita Blvd. at-grade crossing in Arcadia, and ending at Milepost 119.35, just east of the San Gabriel River, in Irwindale.

As part of the historic report required by the STB, BNSF needs to know if there are any structures eligible for listing on the National Register of Historic Places and also if there are archaeological resources in the project area.

The removal of the track associated with this abandonment has already been addressed by the Metro Gold Line Foothill Extension Construction Authority in an Environmental Impact Report and a Supplemental Environmental Impact Report. I'm enclosing a copy of the Executive Summary, the Supplemental Historic Properties Survey and Effects Report (dated September, 2005), and a map of the subject railroad line. The entire reports can be viewed at the following web link:
http://www.foothillextension.org/construction_phases/phase_2a_pasadena_to_azusa/supplemental-environmental-impact-report/2010-draft-supplemental-environmental-impact-report/

Please provide your assessment and comments to me at the address above, if at all possible, by May 13, 2011. You may contact me by email or phone with any questions or concerns.

Thank you in advance for your time and contribution.

Sincerely,

John A. Sims, CP
Paralegal

Enclosures as stated

cc via email: David Rankin – BNSF – david.rankin@bnsf.com
Karl Morell – Ball Janik LLP – kmorell@bjllp.com
Susan Odom – BNSF susan.odom@bnsf.com
Mark Norton – BNSF – mark.norton@bnsf.com
Joyce Chang – LACMTA – changj@metro.net